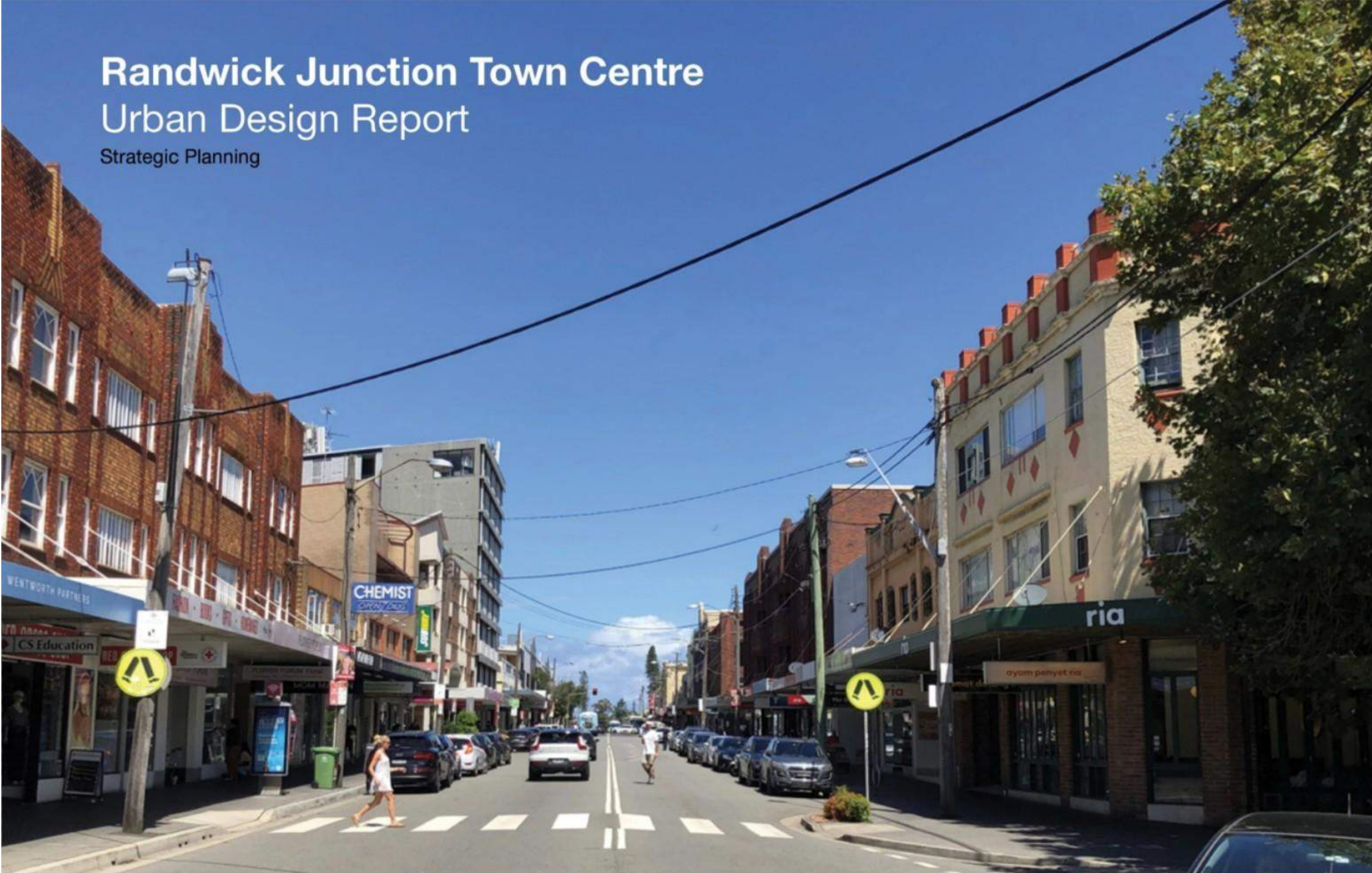


# Randwick Junction Town Centre Urban Design Report

Strategic Planning



File reference	D06964411	Date	Signature
Prepared by	DA + JB + LS		
Coordinator Review	David Appleby	2 December 2025	
Manager Review	Stella Agagiotis	12 December 2025	

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## Acknowledgement of Country

*Randwick Council pays respect to the traditional owners of the land, the Bidjigal and Gadigal people, and we acknowledge the living and continuing culture of the traditional custodians of this country.*

*We recognise that the traditional owners have occupied and cared for this Country over countless generations, and we celebrate their ongoing contributions to the life of the area.*

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# 1. Executive Summary

## Introduction

The Randwick Junction Town Centre (RJTC) Urban Design Report describes Council's strategic planning and urban design approach for the town centre for the next 15-20 years. This report identifies the most appropriate built form response to the town centre's valued heritage significance and 'fine grain' scale, and to the changing context of new public transport infrastructure, new health facilities and the expected population and employment growth - in particular the hospital and university employment hubs. The RJTC is well positioned to leverage the opportunity for innovative health and medical related businesses and to strengthen its role as a commercial centre serving the residential and workforce catchment.

Specifically, the Urban Design Report provides:

- Guidance for the preparation of the Planning Proposal, through built form analysis and recommendations on principal planning standards
- The rationale for the design expectations and massing of future development
- The basis for a future centre-specific Development Control Plan.

## E2 Commercial Centre Zone

RJTC plays an important role as one of the four major town centres within the Randwick City Local Government Area (LGA). RJTC has excellent access to public transport (Light Rail and bus services), offers a variety of retail and is in proximity to the major employment hubs of Randwick Hospital and the UNSW Kensington Campus.

## Population growth and employment

The population of the Randwick Local Government Area (LGA) is projected to grow between 2021 and 2041 by 19,526 people (DPHI Population Projections). The employment projections adjusted for COVID are for an increase of 28,555 new jobs within the Randwick LGA over the 2016-2041 period.

The RJTC is identified, along with the UNSW Kensington and the Randwick Hospital Campuses as the three main areas of projected employment growth in the Randwick City LGA through to 2056 (South East Sydney Transport Strategy, Aug 2020). The retail floor space and employment in RJTC is projected to grow at a slower rate in the short-medium term, and to return to pre-Covid growth rates within the next 8-10 years.

## Local small-scale character

RJTC is valued by the Randwick community for its local friendly 'village' character, cultural heritage, embodied by its Heritage Conservation Area (HCA) status, its many heritage (of both State and Local status) and Contributory Buildings, and civic buildings including Marcellin College, three church precincts, entertainment activities and hospitality venues (Coach and Horses Inn, The Lurline Guesthouse and nearby Royal Hotel). Heritage buildings within the town centre provide physical evidence of staged growth and development of the suburb and the history of Randwick as a city.

Collectively these buildings and nearby parks bring to the streetscapes of the town centre a richness of architectural detail with fine examples of freestanding stone and brick decorative facades and parapets and mature tree canopy in parts of the town centre. Of note is the first statue of Captain James Cook that forms an important landmark at the southern approach to the town centre.

The town centre is also valued by the community for the diversity of offerings including convenient supermarkets and specialist shops, eateries, cafes and community services – the ability for 'one stop shopping'.

## Room for improvement

RJTC is not without its design challenges. There are areas that are 'run down' in appearance, many heritage properties are in a deteriorated state and there is a general lack of appropriate public places to rest and to enjoy within the town centre.

## Movement and Place Framework

The Movement and Place Transport for NSW framework has been considered in the planning review of the RJTC. The Framework balances the movement of people and goods, with pedestrian amenity and the quality of streets as places for people. In relation to street types, the Belmore Road retail main street can be categorised as a Local Street. The ideal character for this type of street is to embody a strong 'sense of place', to provide a high level of walking and cycling amenity, a safe low speed environment, whilst also allowing for the efficient movement of people and vehicles. This includes maximising the road space available for footpaths, street tree planting and alfresco dining and improving pedestrian crossings, to enhance the overall amenity and safety of the streetscape. In relation to Avoca Street, a classified road, can be categorised as a Main Street which is characteristic of an arterial high street.

## RJTC strategy

The RJTC Strategy, endorsed by Council in August 2020, has informed this Urban Design Report which sets out the vision and actions to guide the sustainable growth and physical development for the town centre over the next 15-20 years. The strategy identified public benefits including improvements to the town centre's public domain,

promoted key laneway activation and identified sustainable strategies for environmental conservation and design.

### **Informing specialist planning studies**

This Urban Design Report draws upon past work by Council's Strategic Planning team and by specialist consultants input providing expertise on heritage, financial viability and traffic and transport. It provides the urban planning justification in support of the RJTC Planning Proposal that proposes an update to the Randwick LEP 2012 planning controls that apply to the town centre.

The following specialist consultant studies have informed the urban design and planning approach for RJTC:

- Heritage Assessment – Randwick Junction Town Centre Planning Strategy (City Plan Heritage, Feb 2023)
- Randwick Junction Economic and Feasibility Analysis – Final Report (SGS Economics and Planning, 26 April 2023)
- Local Transport Study – Randwick Junction and HIAs – Final Report (Stantec, Jan 2022)
- Randwick Junction Planning Proposal Flooding Assessment – BMT 2025.

A summary of the key outcomes of these specialist studies is provided in Chapter 3 of this report.

### **Heritage**

The planning strategy's key focus has been to protect existing State and local heritage listed properties and encourage their restoration. The urban planning challenge has been to carefully plan for a moderate uplift on suitable heritage properties, and appropriate uplift on the less constrained strategic sites, whilst also ensuring that any future redevelopment is feasible. In this regard the study was informed by the Heritage Assessment and Economic and Feasibility Analysis.

### **Urban design**

RJTC is unique among the four commercial centres in the Randwick LGA, as the only centre within a Heritage Conservation Area. Its character is defined by over 30 heritage buildings and over 40 contributory buildings.

There is a well-established 'fine grain' urban structure to the town centre, and it is this quality that gives the centre's streets their small scale, variety and interest, and the 'village' like character that the community values.

RJTC is well served by public transport infrastructure, a vibrant mix of retail shopping (including two major shopping centres), local services and the town centre adjoins two

major employment hubs – the UNSW and the Randwick Hospital campus (including an expanded precinct at High Street/Botany Street with three new health facilities). A place-based methodology has been adopted that has identified the strengths and locally distinctive attributes of the town centre and that seeks opportunities for well-planned growth in the centre's commercial, retail and residential capacity over the next 15-20 years.

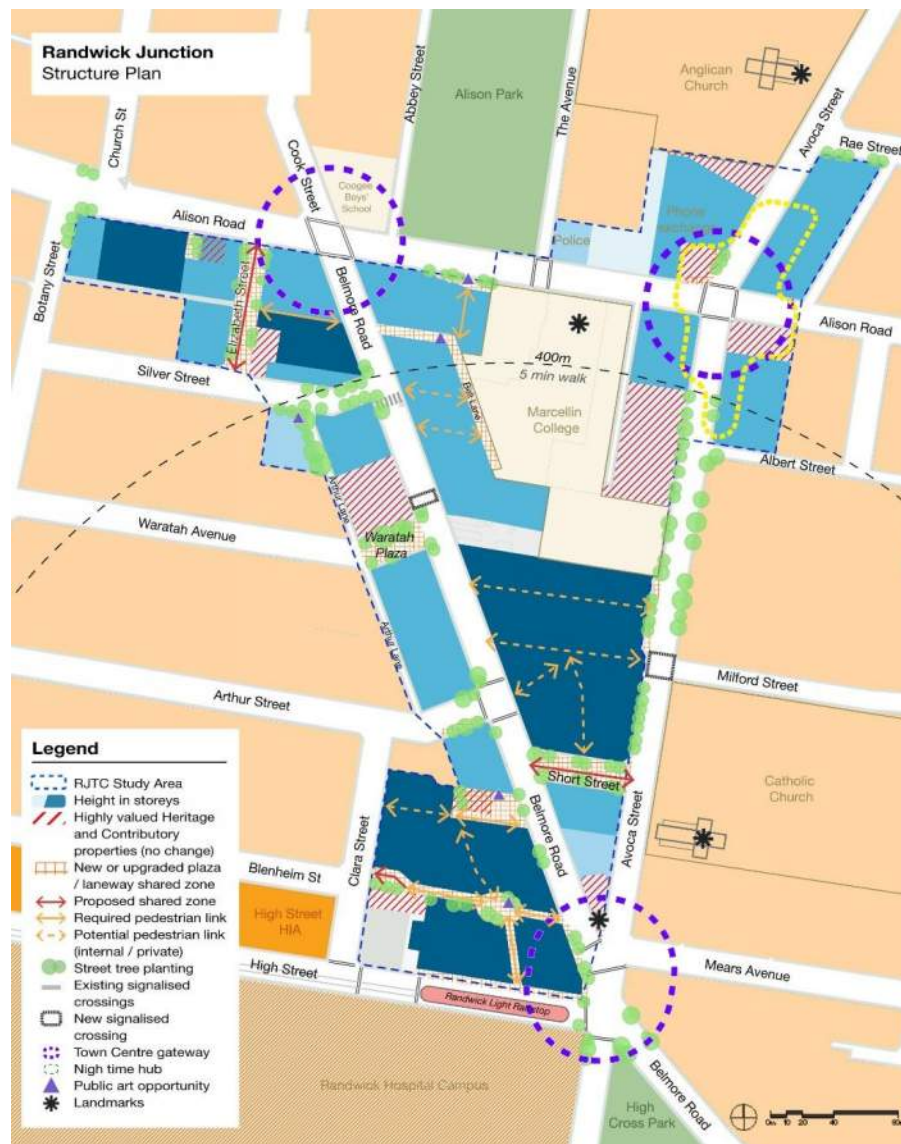
A strategic approach has been taken to the urban planning of the town centre, including looking for opportunities where it is possible to incentivise good urban design outcomes, such as the redevelopment of detracting buildings, the provision of new public places, pedestrianised local streets and laneways and the creation of small urban plazas in public street closures (Waratah Avenue) and in areas of deep soil provision within private development sites.

There are also opportunities for health and medical and innovative start-up businesses to locate in the town centre, leveraging and supporting the growth of Randwick Hospitals Campus and the UNSW. Growth of health, medical and associated uses will strengthen the commercial character of the town centre and add to its vitality.

### **Public benefits and public domain improvements**

A range of public benefits or place-based outcomes have been identified as part of the planning review including the provision of affordable housing, pedestrian through-site links, new plazas, the widening of footpaths, the redevelopment of detracting buildings (identified by the Heritage Assessment) and the restoration of heritage and contributory buildings.





The study has identified a series of potential future public domain upgrade projects that would improve the amenity and attractiveness of the town centre to visitors, workers and residents.

## Previous Community consultation

The RJTC review undertook stakeholder consultation with businesses, institutions, and schools, and a Council webpage (YourSay) was established for the study that reached out to the broader community. This consultation informed the preparation of the RJTC Review Issues Paper and the RJTC Strategy (RCC, 2016).

One-to-one stakeholder consultation was then undertaken from 2020-2022 to inform the preparation of the RJTC Urban Design Report and the RJTC Planning Proposal. The one-to-one meetings with key landowners were undertaken to understand the extent of existing and potential site consolidation that may assist with achieving an optimum urban design outcome, such as ability to introduce new through site pedestrian links and how new built form could be setback to reduce apparent bulk.

The next stage of the planning process for the town centres will include community and stakeholder engagement on the built form and planning controls contained in the Planning Proposal and development control plan.

## Conclusion

This RJTC Urban Design Report recommends to Council a considered and balanced urban planning approach to guide the growth of the town centre over the next 15-20 years.

## Recommendations

The specific recommendations of this study are to:

### Strategic planning

- Encourage renewal at the northern end of the town centre, and bring more activity, by focussing additional height and density for the two Strategic Sites – The Randwick Club and the Former CBA Bank Site – in the north of the town centre
- Incentivise the renewal and renovation of properties at 179-187 Alison Road to create a new outdoor dining destination overlooking Alison Park
- Encourage renewal of Avoca Street, eastern side between Alison Road and Rae Street, and western side between the Belmore Road corner and Short Street by permitting suitable additional height and density for these town centre blocks
- Encourage health and medical and innovative start-up businesses to locate in the town centre, including along High Street, leveraging, and supporting the growth of Randwick Hospital and the UNSW.

### Review of LEP controls

Based on background research and analysis on the existing character, urban structure, built form, future direction, opportunities and challenges within the centre, several LEP controls have been reviewed. The rationale for the LEP changes is addressed in detail in this report.

Rezoning of specific sites at the periphery of the town centre is proposed to accommodate higher built forms, encourage laneway activity, pedestrian improvements and to rationalise the existing town centre business zone.

For State listed and highly valued local heritage listed items, no increase in height or FSR above the existing 12m height control and FSR 2:1 is recommended. Moderate increases in height and FSR are proposed for properties with heritage or contributory buildings (guided by specialist heritage consultant advice) where sites are considered appropriate to accommodate small scale increases of between two and three storeys above the existing height limit (total of 5 or 6 storeys). On the less constrained and larger Strategic Sites, mid-rise uplift in height and density (11-13 storeys) is recommended.

- Zoning – extend the E2 Commercial Centre zone to include four groups of existing R3 zoned sites as per the Proposed Zoning Map
- Floor Space Ratio (FSR) – increase the FSR on those sites indicated on the Proposed Floor Space Ratio Map
- Non-residential FSR – to preserve employment in the town centre, require a minimum non-residential FSR on those sites indicated on the Proposed Minimum Non-Residential FSR Map
- Height of Building (HOB) – increase the HOB for those sites indicated on the Proposed Height of Building Map
- Active Frontages – require active frontages in the main streets of the town centre to ensure street vibrancy, liveliness and pedestrian safety on those sites indicated on the Proposed Active Frontages Map

### Review of DCP controls

Amend the DCP to require:

- All redevelopment proposals within the RJTC of significant scale are to lodge their proposals to the Randwick Design Excellence Advisory Panel (DEAP) for consideration in the assessment of Development Applications (DA).

### Pedestrian access

- Improve the cohesiveness of the overall town centre by incorporating the east-west pedestrian connections and through site pedestrian links between Belmore Road and Avoca Street
- investigate installation of a new signalised intersection at the Avoca Street/Milford Street intersection and facilitate ‘stepping-stones’ of activity on Alison Road and Short Street
- Wherever possible, widen footpaths by requiring building setbacks, and/or reducing carriageways to the minimum standard, to enhance the safety and amenity of pedestrians moving through the town centre

### Public domain upgrades

- June Moore Place – upgrade and renew the landscaping of this important pocket plaza off the Belmore Road Main Street
- East House (former Post Office) – upgrade the footpaths and associated setback areas with new paving, lighting, landscaping, street furniture and heritage interpretive signage providing new green places for people to enjoy in the town centre
- Short Street – One-way shared zone, prioritising pedestrian access and provide outdoor dining areas and new street landscaping, providing new green places for people to enjoy in the town centre
- Elizabeth Street – establish a one-way shared zone, prioritising pedestrian access and provide outdoor dining areas and new street landscaping, providing new green places for people to enjoy in the town centre
- Alison Road dining area – as part of any future redevelopment of these sites, encourage demolition of existing detracting commercial frontages and the restoration of the two storey Victorian houses behind, creating a new north oriented outdoor dining area destination overlooking Alison Park
- Royal Randwick Shopping Centre pedestrian link – as part of any redevelopment of this site, establish generous east-west through site pedestrian links with active frontages, connecting Belmore Road to Avoca Street
- Lighting strategy – Prepare a lighting strategy for the overall town centre, with particular focus on heritage items and contributory buildings along with night-time venues including the two laneways
- Reinforce the new identity of night-time hubs through the coordinated upgrade to the paving, creative lighting and furniture of this northeast area of the town centre.

### Heritage

- If heritage listed buildings or contributory buildings form part of a redevelopment proposal a Heritage Assessment or Heritage Impact Statement is to be prepared, considering the proposed redevelopment proposal prior to any major works being undertaken
- Further, a Conservation Management Plan or Strategy (CMP or CMS) is to be prepared prior to the undertaking of any physical works and as a condition of consent, to ensure the future management and conservation of the heritage item or contributory building is in accordance with Heritage NSW conservation management requirements
- Require the restoration, including the removal of disused or poorly designed/located signage, and the replacement of inappropriate window frames and other details, and repainting of heritage buildings to their original colour scheme based on expert heritage advice
- Preserve heritage or contributory building roofscapes/chimneys
- Strengthen the Randwick DCP development controls, requiring developments to address the maintenance and restoration of heritage and contributory buildings

### Environmental and visual amenity

- Support the Randwick LGA 22% tree canopy target by exploring opportunities for planting appropriate species of street trees where awnings do not preclude plantings
- Explore a minimum percentage Landscape Area requirement for redevelopment in the town centre, similar to that applying in the Kensington and Kingsford Town Centres DCP
- Progressively upgrade the public places in the town centre, including opportunities to incorporate Water Sensitive Urban Design (WSUD) practices and engineering in the public domain
- Consider public artwork opportunities in the public and private domain of the town centre. Including the funding of art in the public domain, in association with public domain improvements
- Plan for the progressive removal of overhead power lines and timber poles, by undergrounding the power supply – consider a new Smart Pole standard for lighting the streets of the town centre

### Nighttime economy

- Encourage the night-time economy, through the renewal of the public domain and existing venues clustered around the Coach and Horses Hotel including

the Avoca Street restaurant strip and former Post Office building and the branding and promotion of the night-time destination

- Support nighttime activation along Belmore Road by upgrading the public domain, plazas and activating shop fronts

### Transport, traffic and parking

- Continue to liaise with Transport for NSW (TfNSW) regarding timing for business case studies into the provision of a Metro line and Randwick Station, and Rapid Bus routes along High Street and Avoca Street, including potential bus-light rail transport interchange
- Investigate implementation of a 30km/h High Pedestrian Activity Area (HPAA) for the Belmore Road main retail street from Avoca Street intersection (south) to the Alison Road intersection (north), in partnership with TfNSW, including appropriate signage and visual cues at the thresholds and traffic calming street design measures
- Investigate the potential to relocate the bus stops from Belmore Road to Avoca Street to improve the pedestrian amenity of the town centre Main Street (Belmore Road)
- Explore opportunities for an additional signalised pedestrian crossing of Belmore Road between Waratah Avenue and Silver Street
- Implement footpath widening and new street trees along the west side of Belmore Road (the east side being impacted by afternoon peak hour parking restrictions) to improve pedestrian safety and amenity
- Liaise with TfNSW and the owners of Royal Randwick Shopping Centre regarding the installation of a new signalised pedestrian crossing at Avoca Street, at or near to the Milford Street intersection, incorporating a southbound right hand turn into the shopping centre carpark, to avoid unnecessary circulation of vehicles via the Belmore Road 'Main Street'
- Develop a public parking management plan for RJTC
- Manage car dependency and ownership over time, by applying reduced car parking rates at one third reduced TfNSW standard rates to redevelopment in RJTC (remaining consistent with the K2K Development Control Plan).

### Flooding

Address the findings of the Randwick Junction Planning Proposal Flooding Assessment (BMT, 2025) that considers the requirements of Ministers Direction 4.1 Flooding



## 2. Introduction

Randwick Junction Town Centre (RJTC) plays an important role as one of the four major town centres (zoned E2 Commercial Centre under the Employment Zone Reforms) within the Randwick City Local Government Area (LGA). RJTC has this status due to its ready access to public transport (Light Rail and bus services), its significant retail offering and its proximity to the major employment hubs of Randwick Hospital and the UNSW Kensington Campus.

The population of the Randwick Local Government Area (LGA) is projected to grow between 2021 and 2041 by 19,526 people (DPHI Population Projections Nov 2024). Considering employment, the adjusted Covid growth projections are for an increase of 28,555 new jobs within the Randwick LGA over the 2016-2041 period.

The RJTC is identified, along with the UNSW Kensington and the Randwick Hospital Campuses as the three main areas of projected employment growth in the Randwick City LGA through to 2056 (South East Sydney Transport Strategy, Aug 2020). The retail floor space and employment in RJTC is projected to grow at a slower rate in the short-medium term, and to return to pre-Covid growth rates within the next 8-10 years.

RJTC is valued by the Randwick community for its diversity and business offerings, local friendly 'village' character, cultural heritage, embodied by its Heritage Conservation Area (HCA) status, its many heritage buildings (of both State and Local status) and Contributory Buildings, and for Marcellin College, three church precincts, and attractive parks.

Collectively these buildings and parks bring to the streetscapes of the town centre a richness of architectural detail with fine examples of stone and brick decorative facades and parapets and to parts of the town centre mature tree canopy. Of note is the first statue of Captain James Cook, along with the heritage buildings on the hospital campus, that forms an important landmark at the south approach to the town centre.

The town centre is also valued for its convenient supermarkets and specialist shops, and community services – the ability to do a 'one stop shop'.

RJTC is not without structural and strategic challenges which over time have the potential to hinder economic growth and vitality and diminish heritage values. There are areas that are 'run down' in appearance, many heritage properties are in a deteriorated state and there is a general lack of appropriate public places to rest and to enjoy within the town centre.

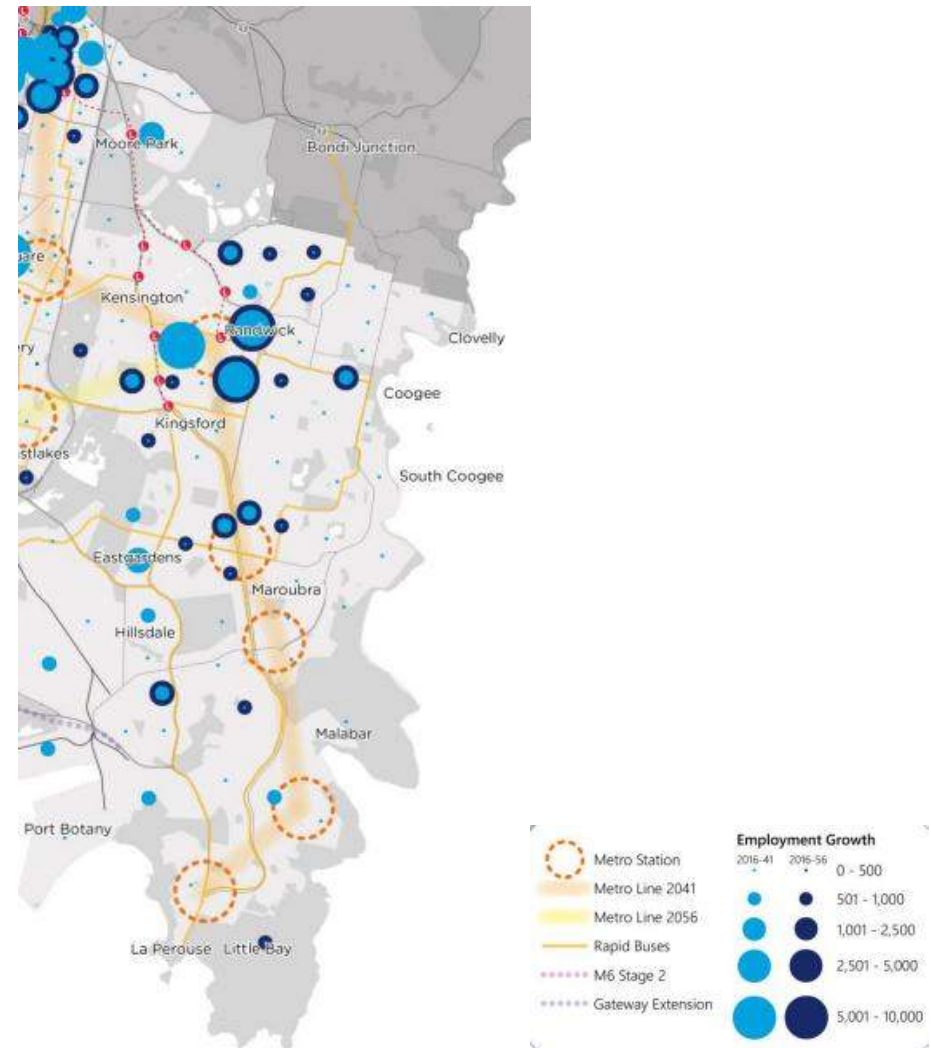


Figure 1: Randwick LGA employment growth



This Urban Design Report draws upon past work by the Strategic Planning team at Council and by specialist consultants providing expertise on heritage, economic analysis, financial viability and traffic and transport. It provides the strategic planning justification in support of the RJTC Planning Proposal that proposes an update to the Randwick LEP planning controls that apply to the town centre.

This RJTC Urban Design Report describes Council's approach to its urban design review and the most appropriate built form response to the inherent heritage scale and 'fine grain' character and unique urban qualities of the town centre. This report also draws on the changing broader context of new transport infrastructure and the accessibility it brings, and within the context of a growing LGA, including hospital and university employment hubs. Specifically, the Urban Design Report serves to:

- Guide the preparation of the Planning Proposal, through built form analysis and recommendations on principal planning standards
- Inform the community, property owners and stakeholders on the design expectations and massing of future development
- Form the basis of a future centre-specific Development Control Plan.

#### **Informing specialist planning studies**

The Urban Design Report is supported by a series of specialist consultant studies that informed the urban design for RJTC:

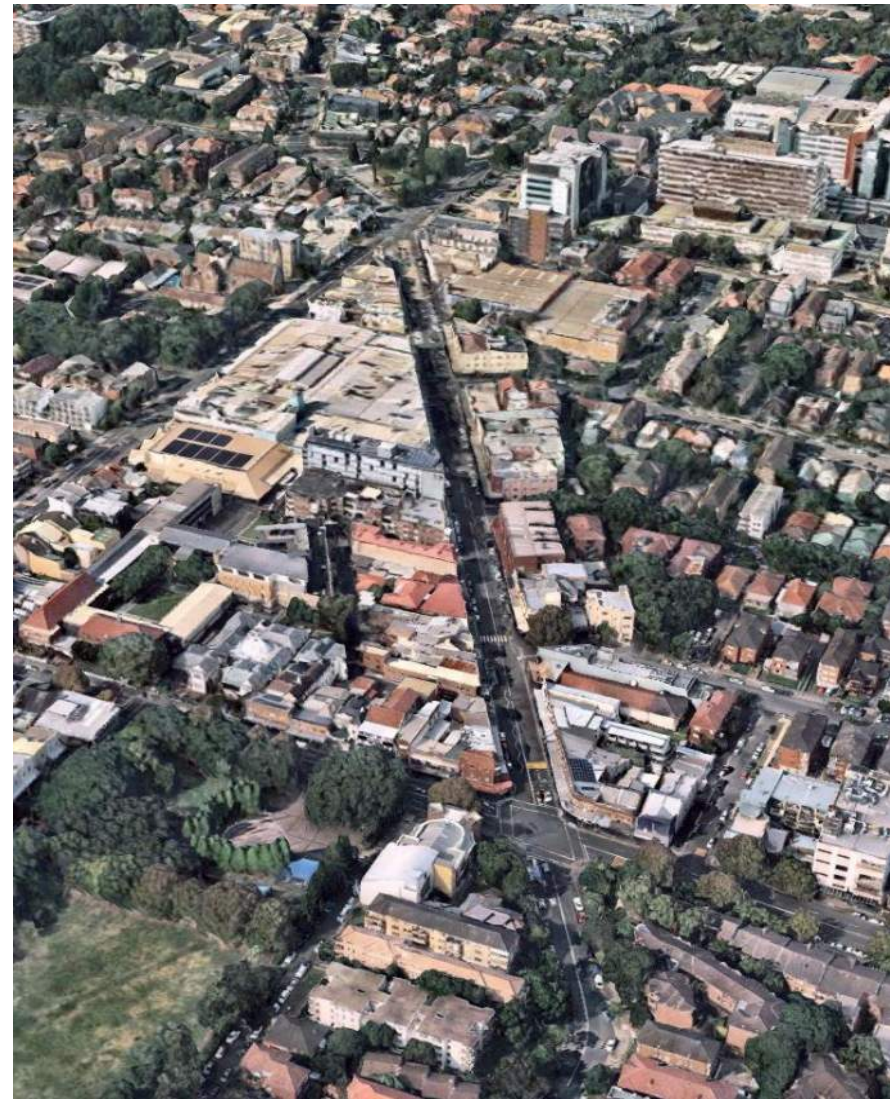
- Randwick Junction Heritage Conservation Review (City Plan Heritage, 2015)
- Addendum to Heritage Assessment for Randwick Junction Town Centre Planning Strategy (City Plan Heritage, 2025)
- Randwick Junction Economic and Feasibility Analysis – Final Report (SGS Economics and Planning, 26 April 2023, and the Randwick Junction Town Centre Development and Affordable Housing Contribution Feasibility Testing (SGS Economics and Planning, July 2025)
- Local Transport Study – Randwick Junction and HIAs – Final Report (Stantec, Jan 2022)

A summary of the key outcomes of these specialist studies is provided in Chapter 3 of this report.

#### **Community consultation**

The RJTC review included stakeholder consultation with businesses, institutions, and schools, and a Your Say Council webpage was established for the strategy that consulted with the broader community. This consultation process was conducted during the preparation of the RJTC Review Issues Paper and the RJTC Strategy (RCC,

2016) that informs this Planning Proposal and report. Outcomes of the consultation are outlined in Section 4.3 of this report.



**Figure 2: Randwick Junction aerial view**

### 3. Strategic planning context

#### 3.1. Metropolitan Plan

The Greater Sydney Region Plan, A Metropolis of Three Cities was released by the Greater Sydney Commission in 2018. The authority was renamed the Greater Cities Commission in Dec 2021 with an expanded geographic remit to address planning of The Six Cities Region, including the Lower Hunter and Greater Newcastle City, Central Coast City and Illawarra-Shoalhaven City.

The Plan sets a 40-year vision and establishes a 20-year plan to manage growth and change for the region. The Plan is built on a vision of six cities where most residents live within 30 minutes of employment, health and education facilities, services, and recreational spaces. Each of the six cities are supported by metropolitan and strategic centres.

The RJTC is located within the Eastern Harbour City, is identified as a Strategic Centre (Health and Education Precinct) in the Eastern City District Plan and forms part of the Randwick Collaboration Area.

#### 3.2. Eastern City District Plan

The Eastern City District Plan sets out the 20-year plan for the Eastern Harbour City, including the planning Directions and supporting Planning Priorities to guide development toward the improvement of the social, economic and environmental assets of the district. The Plan specifies objectives that emphasise the importance of internationally competitive health, education, research and innovation precincts. RJTC's role in this context, given its location as the terminus of the CBD and South East Light Rail route and interface with the Randwick Education and Health Strategic Centre and Randwick Collaboration Area, is to provide a mix of town centre services that support the collaboration area's growth and productivity to improve the liveability for workers, residents and students.

The town centre also capitalises on its convenient access to public transport to create complementary employment opportunities, deliver affordable housing and facilitate improved connections to residential areas.

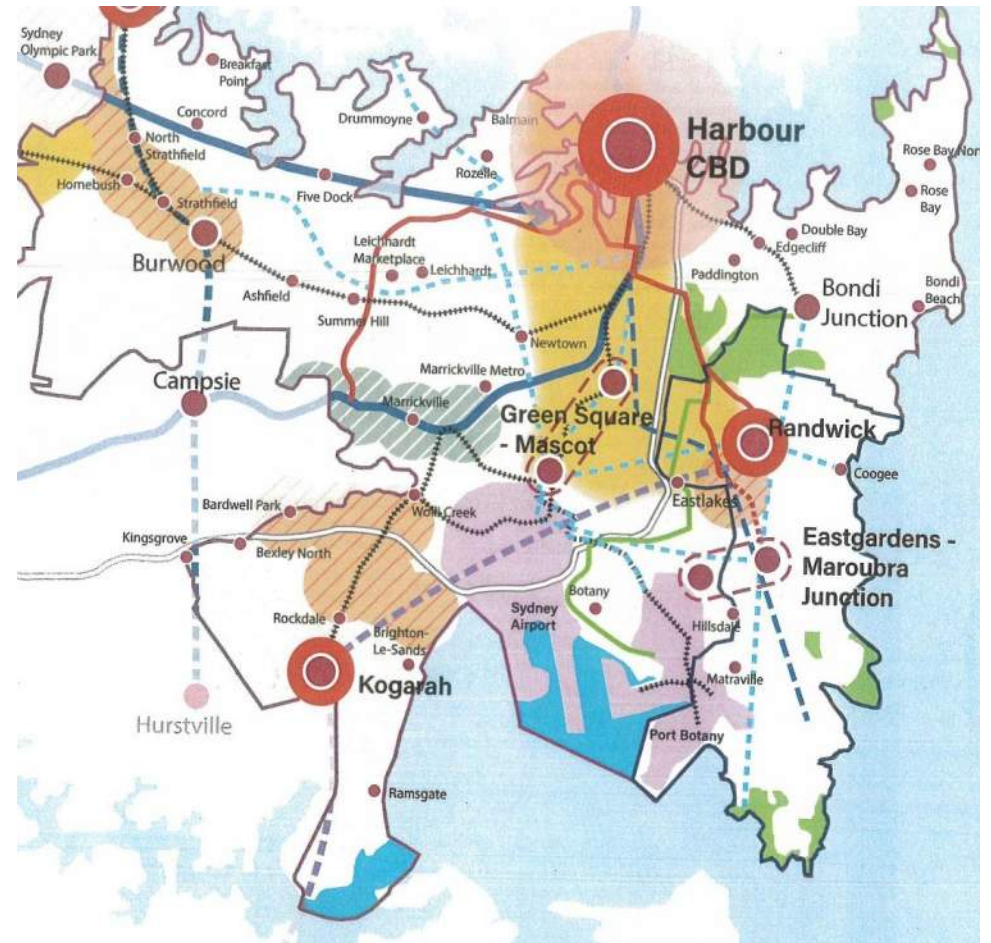


Figure 3: Eastern City District Plan regional context



### 3.3. Local Strategic Planning Statement

The Randwick Local Strategic Planning Statement (LSPS) was prepared in 2020 and provides the framework for land use planning and decision making for the next 20 years. It outlines the community's vision for the future of the Randwick City LGA, and is underpinned by planning priorities outlining where housing, jobs, infrastructure, and open space should be located.

The LSPS gives effect to the Eastern City District Plan, implementing priorities at the local level, by recognising the special character and values that are to be preserved and how change will be managed in the future. The LSPS identifies 23 Planning Priorities and the associated short, medium and long term Actions required for the delivery of the planning priorities, to meet our community's future economic, social and environmental needs and aspirations.

The LSPS identifies Randwick Junction as an important town centre, providing retail, commercial and community services for the surrounding residential catchment. It also performs a support role to the Randwick Health and Education Strategic Centre with complementary health and medical support services, the provision of diverse housing and future opportunities for innovative businesses, creative spaces for start-ups and scale-ups that can take advantage of the physical proximity and connections with existing knowledge-intensive industries. The LSPS identifies opportunities for growth and the strengthening of the Randwick Collaboration Area including:

- Providing an adequate supply of affordable housing to retain low-income workers
- Ensuring adequate floor space capacity to accommodate institutional, business and commercial activities and ancillary health uses
- Promoting and encouraging a more walkable and accessible environment within the Randwick Strategic Centre
- Providing a public domain that connects within the precinct and into the community.

The RJTC Planning Proposal is aimed at facilitating sustainable commercial and residential growth to support the Randwick Education and Health Strategic Centre in line with the Planning Priorities and Actions of the LSPS.

### 3.4. Randwick Integrated Transport Strategy

The Randwick Integrated Transport Strategy is a 10 year plan to help deliver Council's longer term vision contained in its City Plan through specific objectives and strategic approaches. The objectives are to encourage a safe, efficient and sustainable road and transport network, and a parking system to cater for all members of the community. Objectives include:

- Increase the active transport mode share to 35% by 2031, from a 26% baseline.
- Reduce the proportion of private vehicle trips from the 2018-19 baseline of 58% to 45% by 2031.
- Achieve a 50% reduction in casualties on the road network from a 2018 baseline of 269 incidents by 2031.



Figure 4: Randwick City Structure Plan

### 3.5. Collaboration Area – Randwick Place Strategy

The Randwick Collaboration Area contains the University of NSW Kensington and Randwick Campuses, Randwick TAFE, the Randwick Hospitals Campus and the recently completed Campus Expansion Area, the Kensington and Kingsford Town Centres, the RJTC, The Spot and the Royal Randwick Racecourse.

The Strategy was prepared by the Greater Sydney Commission (GSC) in 2018 and brought together the main stakeholders, including Randwick City Council, to establish a collective vision for the Randwick Collaboration Area. The vision for the Randwick Collaboration Area is:

*‘By 2036, Randwick has matured into an innovation district of engaging places, with a highly integrated university and health campus. Town centres, residential, employment, recreation and community areas are interconnected, allowing people to move, interact and share knowledge and ideas.’*

The strategy identifies impediments, priorities and opportunities as well as the actions required to deliver the vision. One of the key drivers for the Randwick Collaboration Area is the Randwick Health and Education Precinct which includes the university and hospital. The strategy aspires to develop an internationally competitive innovation precinct with interrelated health and education assets, surrounded by a network of medical research institutions, a mix of complementary industry tenants, housing, ancillary facilities and services.

The proximity of RJTC to the hospital and university employment hubs, the progressive return of international students to the university and the ready access to reliable public transport suggests that the town centre can leverage these locational advantages with businesses, services and accommodation that complements the core hospital and university role and functions. The town centre businesses that will prosper in the future will be the ones that adopt a collaborative mindset and understand the broader planning context.

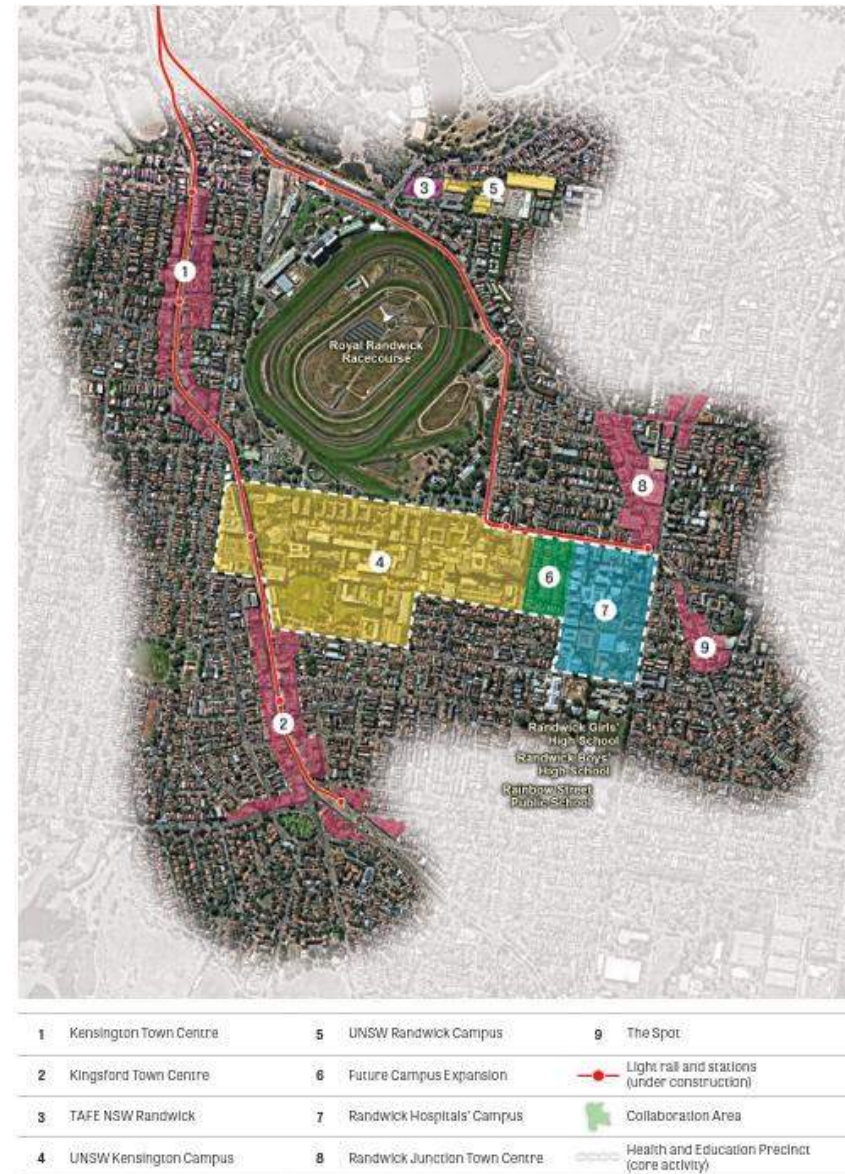


Figure 5: Randwick Collaboration Area key sites map



### 3.6. Randwick Hospital Campus Masterplan

Randwick Hospital campus is in the process of expanding with three large new health facilities (refer to Figure 6 and 7):

- Integrated Acute Services Building (IASB)
- UNSW Health Translation Hub (HTH)
- Sydney Children's Hospital Stage 1 (SCH1) and Children's Comprehensive Cancer Centre (CCCC) building

This will increase the number of hospital employees (doctors, nurses, specialist technicians, medical researchers, clinicians, and support personnel) and patients being located within convenient walking distance of RJTC, supporting growth in business in the town centre.

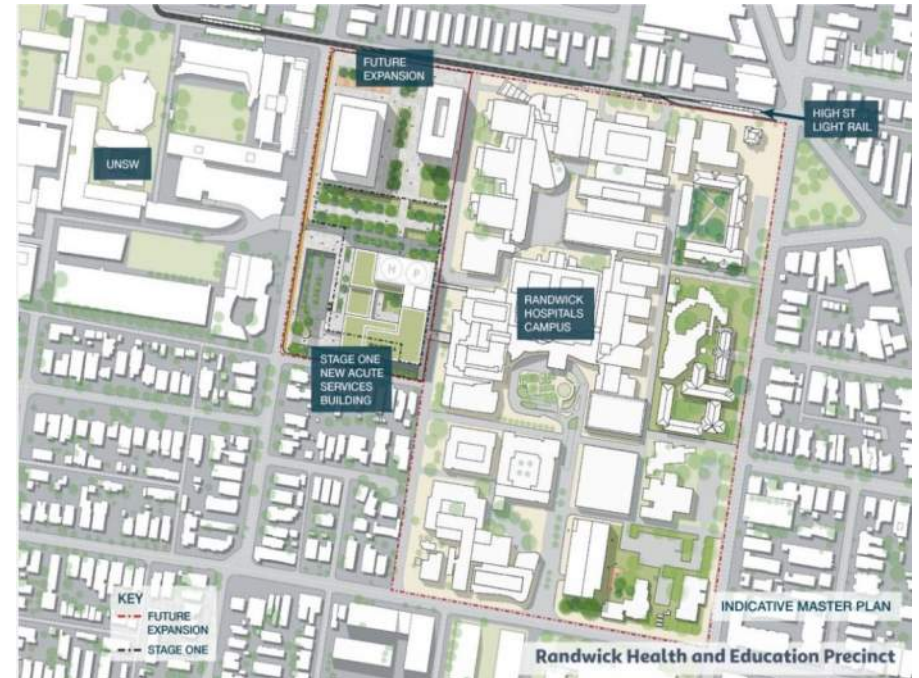
The Randwick Health Campus Masterplan envisages a continuing staged redevelopment of the campus hospitals through to 2045, including the redevelopment of the Prince of Wales Hospital, the Royal Women's Hospital, and the Sydney Children's Hospital. The proposed redevelopment will act as a catalyst for the creation of up to 15,000 new jobs, providing a major level of economic stimulus.

The UNSW with over 63,000 students and over 7,000 academic and administrative staff, and the Randwick Hospital campus with approx. 3,000 staff and 450 beds, located immediately to the south of the town centre, provides an important source of growth for the centre's retail and commercial businesses and community services.

Complementary health support services, such as dentists, chemists, GP and specialist doctors/consultants, medical services, diagnostic facilities and short stay accommodation are already located in the centre and are envisaged to develop further over the next decade, supporting the planned growth of the hospital and university.



**Figure 6: Integrated Acute Service Building, UNSW Health Translation Hub and Sydney Children's Hospital Stage 1**



**Figure 7: Randwick Health and Education Precinct Masterplan**



### 3.7. Randwick Junction Planning Strategy (2020)

The Randwick Junction Planning Strategy (endorsed in August 2020) sets out the vision and actions to guide the sustainable growth and physical development of the RJTC over the next 15-20 years. The strategy was informed by considerable background research and analysis as well as extensive community engagement to formulate the key drivers and improvements for the changing town centre. The planning strategy has informed this RJTC Urban Design Report which responds to several drivers and challenges outlined below.

#### District planning priorities

The Eastern City District Plan (ECDP) sets out a 20-year plan and 40-year vision for the East District, which includes, 'Internationally competitive health, education, research and innovation precincts'. One of these precincts is designated as the Randwick Strategic Centre which includes the RJTC. The ECDP has a projected job target of about 35,500 jobs by 2036 for the Hospital & University Campus. The future development and growth of this precinct will generate a demand for more floor space for retail/commercial/medical uses.

RJTC's role in this context, given its location as the terminus of the CBD and South East Light Rail route, and interface with the Randwick Education and Health Strategic Centre and as part of the Randwick Collaboration Area, is as a mixed-use precinct that will support the Collaboration Area's growth and liveability for workers, residents and students.

#### Strengthening heritage character

A large part of the RJTC is located within the Randwick Junction Heritage Conservation Area (HCA), and a significant number of properties within the town centre are heritage listed (RLEP 2012). The strategy values and recognises the heritage qualities by ensuring future growth is directed into select Strategic Sites where larger buildings can be accommodated, and new development can be designed to integrate with the existing heritage buildings and small scale fabric of the town centre.

#### Strategic Sites

Strategic sites have been identified within the town centre that are in proximity to transport and key infrastructure, and provide significant opportunity for improvement and renewal. Further fine grain controls for strategic sites have been included within this Urban Design Report.

#### Public domain improvements

The Randwick Junction Planning Strategy recognised and outlined improvements to public spaces, encouraging interaction between people and activated spaces. This Urban Design Report builds on the spaces recognised within the 2020 strategy, providing concept plans and controls for further opportunities including outdoor dining, footpath widening, plazas and open space. Further public domain improvements can be found in Section 5.5 of this report.



Figure 8: Randwick Junction Planning Strategy 2020



## 4. Analysis

This chapter provides an urban planning overview and analysis of the existing Randwick Local Environment Plan (LEP) planning controls and identifies the key planning issues facing RJTC in the next 10-15 years. The chapter concludes with a constraints and opportunities analysis.

The RJTC is one of four E2 Commercial Centre zones within the Randwick LGA. The E2 zone is in two parts – along the main retail street Belmore Road and clustered around the Avoca Street/Alison Road intersection.

### 4.1. Principle planning controls

The following Randwick LEP planning controls apply to the RJTC:

#### 4.1.1. Land zoning

The current zoning of RJTC, under Randwick LEP 2012, is primarily E2 Commercial Centre: Special Purposes (SP2) zoned areas include the Alison Road and Avoca Street corridors (which are both classified as State roads), Marcellin College in the middle of the town centre, church owned land to the north and east of the town centre and Randwick Hospital to the south. Small areas at the periphery of the town centre are zoned R3 Medium Density Residential. Figure 9 shows the existing zoning within the RJTC and surrounding areas.

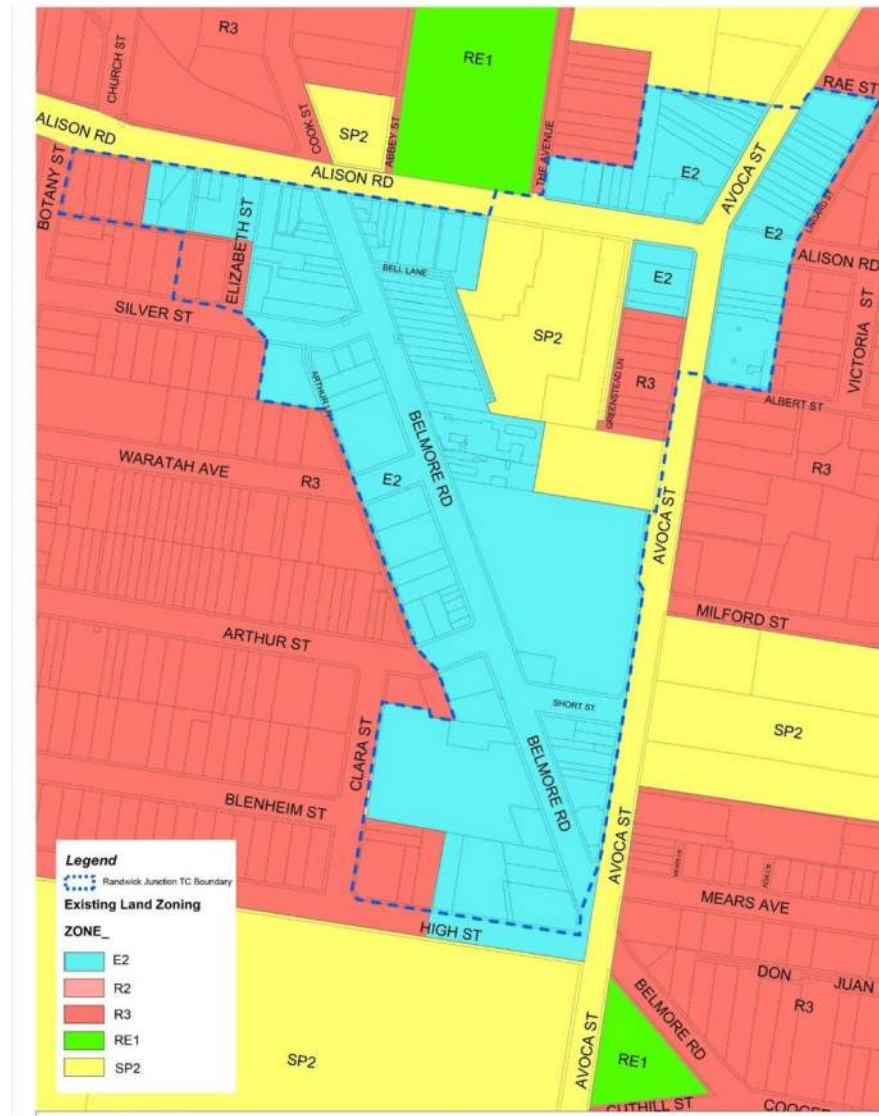


Figure 9: Existing Land Zoning map

#### 4.1.2. Floor Space Ratio

Generally, the E2 Commercial Centre zoned properties within RJTC have a maximum permitted Floor Space Ratio (FSR) of 2:1. R3 Medium Density Residential zoned properties at the periphery of the town centre have a maximum density of FSR 0.9:1.



Figure 10: Existing FSR map

#### 4.1.3. Height of Buildings

All land within the RJTC, except for Marcellin College, has a maximum Height of Building (HOB) control of 12m under the RLEP 2012.



Figure 11: Existing Height of Building map



#### 4.1.4. Heritage

The Randwick Junction Heritage Conservation Area (HCA) extends across most of the town centre (see Figure 12). The town centre contains two State Heritage registered items – I301 Sandgate Cottage at 128 Belmore Road and I274 Easts House at 124 Alison Road, the former Post Office. The Randwick LEP 2012 identifies 31 Heritage Listed properties in the town centre.

There are 47 Contributory Buildings in the Randwick Junction town centre. The significant heritage and contributory built fabric of the town centre establishes the predominantly small scale and ‘fine grain’ urban character of the town centre’s streetscapes and public places.

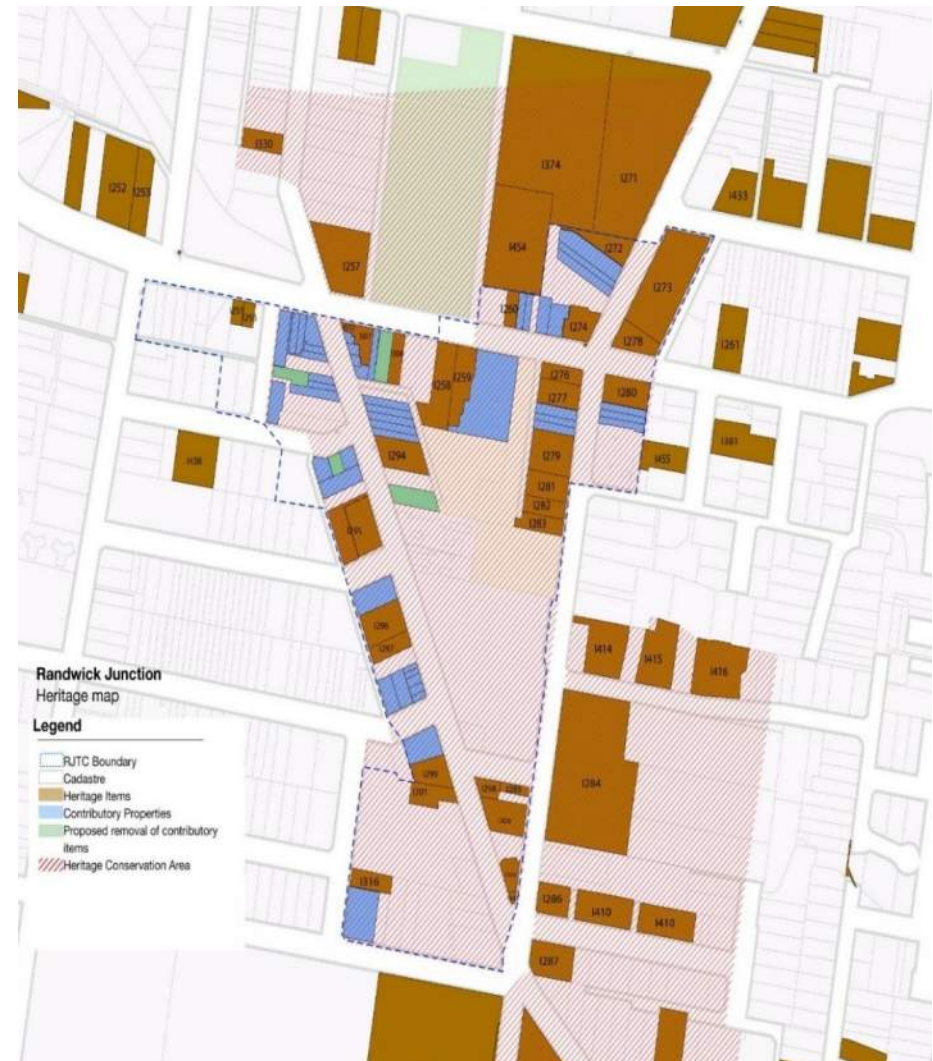


Figure 12: Heritage map

## 4.2. Transport

The Eastern City District Plan recognises the opportunity to integrate public transport with car sharing, rideshare and other emerging sustainable modes to help connect residents to town centres within a 30-minute walking catchment. Active transport, including walking and cycling, accounts for a major method of access to and from RJTC. In a Your Say Randwick Survey in 2017, almost half of all people visiting RJTC make the journey by active modes of transport, with 41% of survey respondents walking and 5% cycling. Approximately half of all visitors to the town centre live less than one kilometre away.

### Future Transport 2056

The NSW Government's Future Transport 2056 study addresses the important role of local centres in supporting a 30-minute city, as they are a focal point of neighbourhoods and transport interchanges. Transport for New South Wales (TfNSW) is committed to supporting councils to apply the 'Movement and Place Framework' (the Framework) which balances the movement of people and goods, with pedestrian amenity and the quality of streets, as places for people. In relation to street environments, the Framework identifies the following street types:

- **Places for People** are at the heart of communities and provide people orientated street environments. The Framework identifies the need to better prioritise public transport, pedestrians, and cycle access whilst limiting through-traffic with no destination in the centre.
- **Local Streets** set the frame for communities, pedestrian amenity and the need for local access as a priority. The Framework identifies the need for local streets that are safe environments, supported by lower vehicle speeds that better align with the need to prioritise walking and cycling within the local community.
- **Vibrant Streets** need to balance high pedestrian activity and densities, attracted by significant commercial, tourism, leisure, and entertainment venues along with the need to move high numbers of people and goods.
- **Movement Corridors** and motorways are important for the movement of people and goods with little interaction with adjacent land uses, as such there is a low priority and need to provide for pedestrians and access to adjoining land uses.

Under the Framework, the Belmore Road main retail street is classified as a Local Street, highlighting the need to improve the 'sense of place', to provide good walking and cycling access, a safe, low speed environment, whilst also allowing for the efficient movement of people and vehicles. This street type includes the reallocation of road

space to improve pedestrian amenity and safety, including the widening of footpaths, and providing opportunities for street trees and alfresco dining.

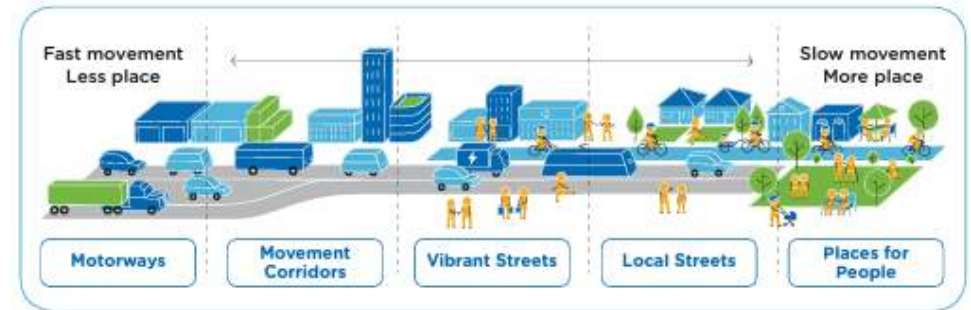


Figure 13: Movement and Place Framework

### South East Sydney Transport Strategy 2056

The strategy was prepared by the NSW Government in August 2020 and provides a long-term outlook for transport planning for the region for the next 30 years. The preferred scenario, based on population and employment forecasts to 2041 and 2056, identifies Metro and Rapid Bus initiatives to be investigated (subject to business cases) to serve the Randwick LGA population in the future.

If the Metro line is found to be feasible, a potential Metro Station is envisaged just south of the High Street and Wansey Road intersection. If the Rapid Bus proposal proceeds, bus routes are envisaged north-south along Avoca Street and east-west along High Street. Points of interface with the current Light Rail Stations would also require careful consideration and planning.

The Transport strategy's Principal Bicycle Network aligns with Council's Bicycle Route Construction Priority (2015) map, with the east-west Coogee Beach to Randwick/UNSW bicycle route identified to proceed along High Street.

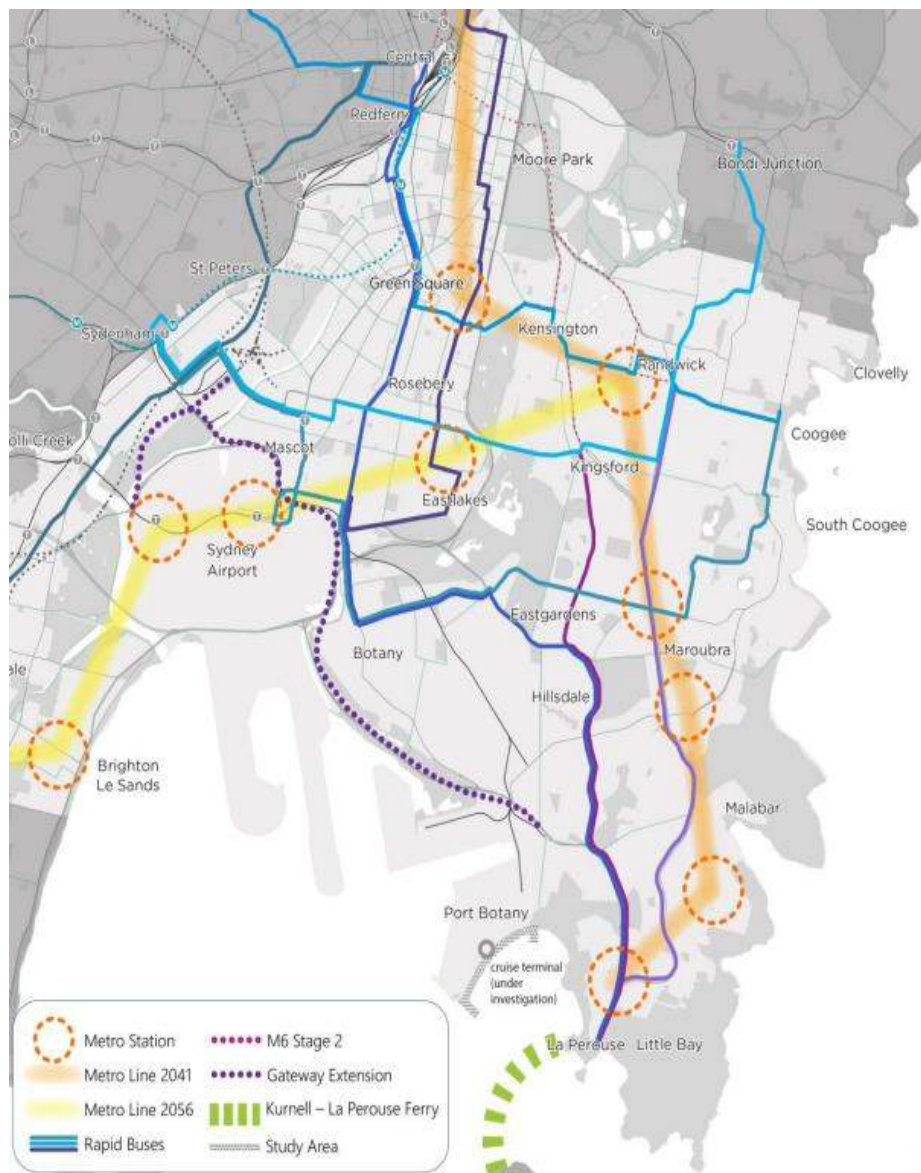


Figure 14: South-East Sydney Transport Strategy

### Local Transport Study – Randwick Junction and HIAs

A Local Transport Study (LTS) was commissioned by Council to inform and guide the planning review. The study, was prepared by Stantec in Jan 2022 and recognises Council's objective to develop a safe, efficient, and sustainable multi-modal transport network that incentivises sustainable travel choices and reduces car dependency.

The study investigated the existing transport facilities and conditions for public transport, active transport, private vehicles and car parking within and in the vicinity of RJTC. The study also assessed the current planning and design controls contained within the Randwick DCP 2013 and assessed the implications of population growth and development in and surrounding the town centre. The study provides a series of recommendations for traffic, transport and public domain improvements and for new infrastructure provision.

The study recommends the following:

- Co-ordinate with Transport for NSW (TfNSW) to regularly monitor traffic performance and investigate improvements at the Alison Road/Avoca Street intersection
- Prepare a traffic circulation plan for RJTC, seeking to distribute traffic more effectively according to each road's capacity, as well as Council's other non-traffic related objectives for particular streets
- Develop a comprehensive public parking management plan for the RJTC, setting the 'trigger' conditions aligned with the 'graduated parking management approach'
- Co-ordinate with TfNSW in the investigation of a high-quality transport interchange – a holistic solution accounting for all users (light rail terminus, 2 x rapid bus stop pairs, cycleways, vehicle traffic, etc.)
- Ensure that new high-density developments incorporate laneways and shared zones where possible to prioritise movement by people on foot
- Upgrade zebra crossing at Belmore Road/Silver Street to a raised crossing
- Investigate the feasibility of a signalised crossing at Avoca Street (either at or north of Milford Street), to enable new turning movements into the Royal Randwick Shopping Centre car park and to improve traffic circulation in the RJTC
- Conduct walking and intersection crossing counts at key locations on Belmore Road, in order to obtain baseline pedestrian data to inform potential future project delivery (e.g. road space reallocation)
- Commence cycleway concept design process for Principal Bicycle Network (noting the requirement to involve TfNSW for Avoca Street, given the State road designation) routes that interface with the town centre, due to their high complexity and interaction with other road users



- Reduce car dependency and car ownership rates, amend the Randwick DCP to apply new parking rates to the RJTC, at one third reduced rates on the TfNSW standard parking rates (that mirrors K2K controls).



**Figure 15: Local Transport Study recommendations map**

### Active Transport Plan (ATP)

The ATP was endorsed by Council in December 2024, includes a statement of vision, and establishes six guiding principles: Safe, Direct, Connected, Attractive, Comfortable and Adaptable. It outlines the long-term plans for walking and cycling in the LGA, and has a series of objectives and priority actions.

### Future work

Council has commissioned an update to the Local Transport Study, focussing on Randwick Junction, that will be completed in late 2025/early 2026.

### 4.3. Feasibility analysis

Council commissioned SGS Economics and Planning (and previously Hill PDA) to provide expert advice and feasibility analysis to inform the planning of RJTC. Seven Test Sites were established that included five of the Strategic Sites (Sites 1, 2, 8, 5b and 5a) and two further sites that would represent typical development conditions along Belmore Road and Alison Road (Sites 6 and 7).

The Urban Design study has established the maximum building height and overall built form envelope that would be appropriate for each property in the RJTC, based on an assessment of heritage, contributory and streetscape scale considerations.

Extensive 3D modelling of the town centre was undertaken to test various scale and setback scenarios. The modelling assisted in visualising and establishing the optimum overall built form and public domain for the town centre. SGS were provided with the estimated yield for each property, based on the 3D modelling, including the Gross Floor Area (GFA) and the resulting Floor Space Ratio (FSR).

The percentage levy applicable for properties in the RJTC is based on the advice provided in the Randwick Junction Economic and Feasibility Analysis report (SGS Economics and Planning, Final Report, 26 April 2023 and 2025 updates).

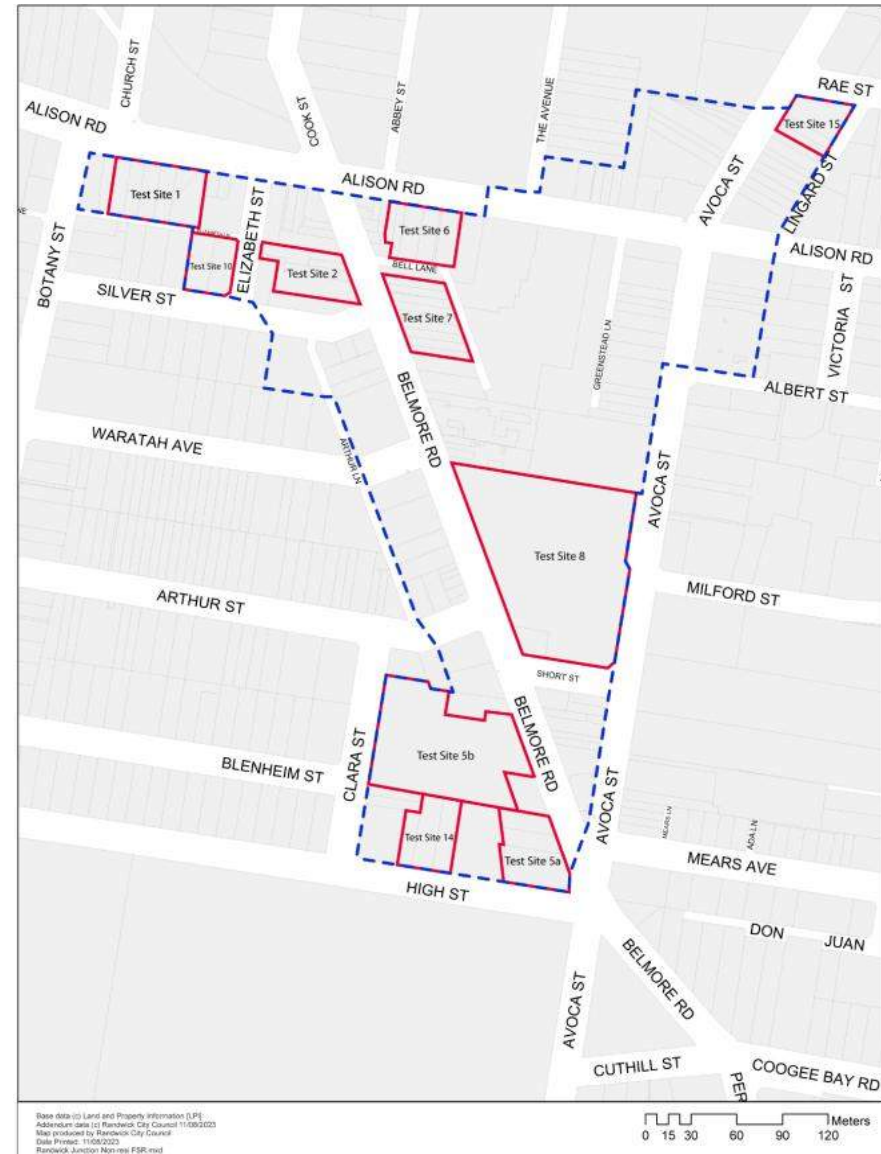


Figure 16: Location of test sites

## 4.4. Urban design analysis

This section of the report considers the quality of the existing public places within and adjoining the RJTC. The quality of life offered by the town centre, for visitors, workers and residents, is closely related to the design quality of the town centre streets, squares and parks – known as the ‘public domain’.

### 4.4.1. RJTC public domain

The existing town centre has public places that provide both hard paved urban plazas and soft shaded green places to rest and relax. These places within or at the periphery of the town centre are landscaped and often incorporate heritage items, mature tree planting and associated landscaping, offering at different times of the day, resting points for shoppers/visitors, workers and residents.

#### Alison Park and High Cross Park

The two parks are located at the north and south extent of RJTC and provide a backdrop of greenery and shade, places for people to walk and to sit, and areas for children of various age groups to play. The parks are utilised by employees, local schools and the public. There are many large established trees (Norfolk Island Pines, Fig Trees, Plane Trees, etc.), playgrounds, picnic tables, seating, drinking fountains and public toilets.



Figure 17: Alison Park



Figure 18: High Cross Park

#### June Moore Place

This plaza is in the front setback from Belmore Road to the state heritage listed Sandgate house. A feature of the plaza is a two-storey high mural dedicated to six notable members of the local community. The plaza is run down and uninviting, and the interface with the Randwick Plaza Shopping Centre is poorly resolved. There is a lack of inviting seating (public or private) and the current fence tends to divide the space from the street activity. Opportunities for renewal of this place are identified in section 5.5 – Public improvements of this report.



Figure 19: June Moore Place



### East's House

East's House (formally Randwick Post Office) is an existing State and local listed heritage item, located in the northeast of the town centre, on the northwest corner of the Alison Road/Avoca Street intersection. This small plaza is paved and has bench seats, interpretive signage and landscaping.



Figure 20: East's House

### Belmore Road 'Main Street'

Belmore Road is the primary retail shopping street for the town centre. It is currently dominated by vehicular traffic and buses. Hard paved surfaces predominate, raising temperatures during summer due to the urban heating effect.



Figure 21: Belmore Road streetscape

## 4.4.2. View analysis and landmarks

### Captain Cook statue

There are several landmarks and prominent buildings that make a considerable contribution to the visual character of RJTC. The Captain Cook statue and castellated sandstone tower are located on the prominent corner of Belmore Road and Avoca Street and mark the east approach to the town centre. The statue was presented to Council in 1910. It is a heritage listed site and forms an important landmark within RJTC. Located directly behind the statue is the former Star and Garter Inn which was one of the earliest hotels in the town centre, marked by its castellated sandstone tower, originally utilised as a viewpoint overlooking Botany Bay.



Figure 22: Captain Cook Statue and former Star and Garter Inn

### **Coach and Horses Hotel**

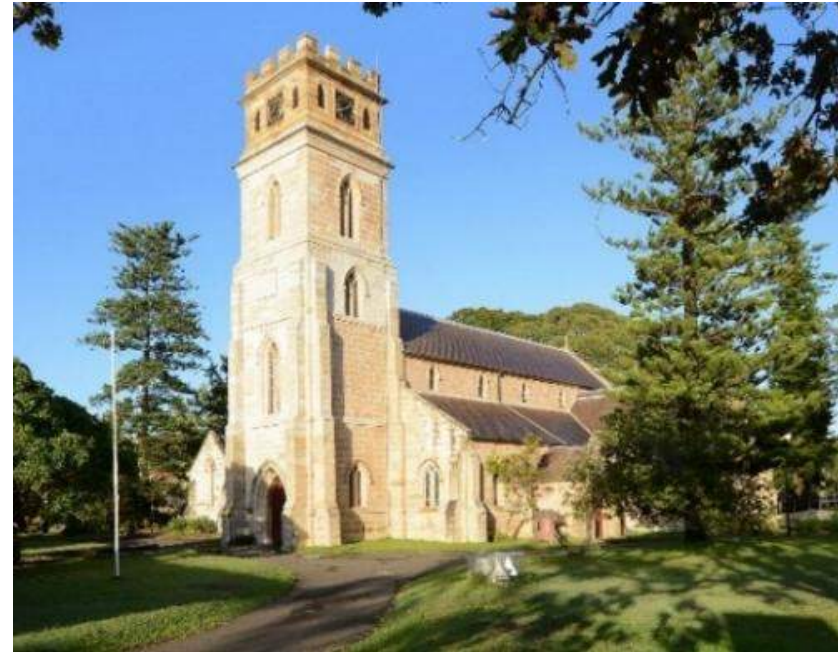
The hotel is a significant landmark within RJTC. Prominently located on the corner of the intersection of Alison Road and Avoca Street, the hotel originally operated as a general store in 1859. The hotel is a locally listed heritage item and is a popular night time venue.



**Figure 23: Coach and Horses Hotel**

### **The churches**

The Our Lady of the Sacred Heart Church, St Jude's Church and Randwick Presbyterian Church all form landmarks visible from various vantage points in the public domain of the town centre. These landmark spires and towers contribute to the character of RJTC and the surrounding residential areas. New development needs to respect the existing vantage points and view corridors for these important wayfinding and symbolic landmarks.



**Figure 24: St Jude's Church Randwick**



## 4.5. Heritage considerations

A large part of the RJTC is located within the Randwick Junction Heritage Conservation Area (HCA), and a significant number of properties within the centre are heritage listed (RLEP 2012), or are identified as contributory buildings (Randwick DCP 2013). The significant extent of heritage fabric within RJTC requires a nuanced and 'fine grain' planning approach.

Heritage considerations are important in the redevelopment of sites within the town centre. Generally, a moderate uplift in building height and in the density (FSR) of development is therefore considered appropriate to address future employment and residential demand. Four larger consolidated sites, that are less constrained by heritage considerations were identified in the RJTC Strategy as Strategic Sites. The Strategic Sites have the potential to accommodate increased height and density due to their larger size and less constrained site conditions.

The larger size of the Strategic Sites facilitates better urban outcomes as there is more scope to transition building height to adjoining properties. On these sites, Apartment Design Guide (ADG) requirements can more easily be met – for deep soil areas, building-to-building setbacks, the provision of access to sunlight and cross ventilation. Vehicular access and servicing can also be consolidated, thereby reducing the overall impact on the public domain and increasing the amenity for apartment residents.

Expert heritage advice was provided to Council by City Plan Heritage, and this has informed the RJTC Strategy, and the preparation of this Urban Design Report for the town centre. The expert heritage planning advice has guided the detailed planning of sites within the town centre where heritage considerations apply. A nuanced and creative urban design approach is required to protect and enhance the cultural heritage of the town centre, including the identified significant built fabric, for future generations to enjoy.

City Plan Heritage has provided advice and review of various increased height and density (FSR) scenarios, and on development approaches that preserve the integrity of the heritage fabric of individual buildings and of the town centre as a conservation area, whilst allowing a moderate level of redevelopment to occur.

Creative planning controls that incentivise the restoration and adaptive reuse of heritage fabric and other significant buildings within the town centre have been explored with City Plan Heritage and the outcome of workshop sessions have been incorporated into the urban planning of RJTC.



**Figure 25:** Our Lady of the Sacred Heart Catholic Church, Avoca Street



## 4.6. Key issues

The RJTC has developed along three primary street arteries – Belmore Road, Alison Road and Avoca Street. The main retail shopping street is Belmore Road that attracts the largest numbers of pedestrian traffic and activity. The following urban design issues have been identified through the analysis:

### Segregation of activities & connectivity

A key structural issue for the centre's business activity is that it tends to be divided into two parts. The main cluster along the Belmore Road 'main street' spine, and a second cluster of businesses around the busy Alison Road and Avoca Street intersection.

Currently the two larger parcels in the middle of the town centre – Royal Randwick Shopping Centre and the Marcellin College campus – present limited potential for east-west pedestrian connections and for the continuity of street level activity between the two parts of the centre.

The urban design strategy seeks to improve the cohesiveness of the town centre through better connecting areas of activity and linking up the commercial functions of the centre into a more comprehensive whole. The urban design analysis identifies the need for more permeability within the centre, improved east-west pedestrian connections (between Belmore Road and Avoca Street) and a review of signalised crossings of Avoca Street to improve pedestrian safety and access to the centre from the medium density residential areas to the east. Improving circulation within and to RJTC and improving the continuity of active frontages will realise a more cohesive and integrated business centre.

The urban design study recommends that any redevelopment of key Strategic Sites, such as the Royal Randwick Shopping Centre, include improved east-west pedestrian connections as part of the proposal. New traffic signals should also be installed at the Avoca Street/Milford Street intersection to further improve pedestrian safety and connectivity.

The creation of new pedestrian places at Short Street and on the south side of Alison Road shown green on Figure 26 will provide 'stepping stones' of activity that will assist to connect the two parts of the centre and provide more continuity of street level activity. This is further addressed in Section 3.7 Constraints and Opportunities.

Strengthening the existing night time activity hub at the Avoca Street/Alison Road intersection as well as building on the existing presence of the Coach and Horses Hotel, will provide a renewed destination and opportunities to draw people to the northeast corner of RJTC that is currently run down and lacks general activity.

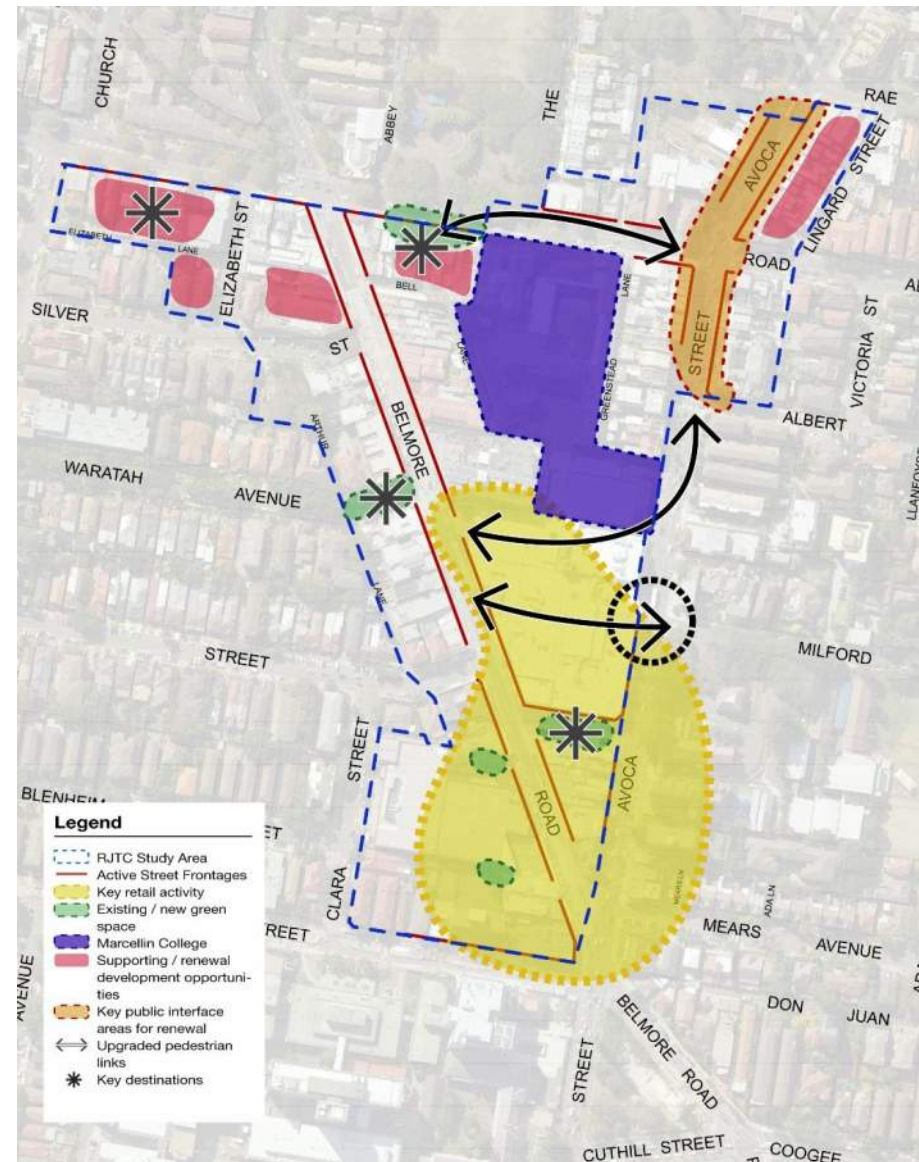


Figure 26: RJTC Key Structural Moves

### **Belmore Road spine – lack of retail activity**

The study identified that commercial activity centred along the Belmore Road retail ‘spine’ diminishes in quantity and quality in the northern city blocks. The south part of Belmore Road benefits from the two large shopping centres, proximity to the Randwick Hospital employment hub and the Randwick stop of the Light Rail, whilst the north part of Belmore Road currently lacks a commercial magnet and public spaces that would draw activity to the north of the street.

The urban design strategy addresses this issue through:

- Identifying the need for additional height and density on two drawcard destination Strategic Sites:
  - The Randwick Club
  - Former CBA Site (16-24 Belmore Road and 4 Elizabeth Street)
- Including new public realm improvements in the planning for the town centre:
  - Footpath widening along sections of Belmore Road and Alison Road
  - June Moore Place upgrade
  - Elizabeth Street shared zone
  - Bell Lane shared zone
  - Arthur Lane shared zone
- Extending the town centre boundary to include properties along Alison Road, Elizabeth Street and High Street. Establishing a shared zone in Elizabeth Street and providing the opportunity for increased height and density to bring more activity and residents to the north of the centre.
- Alison Road Alfresco Dining Strip – Incentivising the removal of detracting buildings along Alison Road (as advised by City Plan Heritage) and the restoration of the original Brisbane Estate Victorian houses for restaurant/café uses with north oriented outdoor dining areas along the Alison Road frontage, overlooking Alison Park. Additional height for apartment/hotel development at the rear of the properties can incentivise the renewal and improved streetscape and public domain outcomes.

### **Avoca Street – run down appearance**

The study has identified that commercial activity along the east side of Avoca Street, particularly north of Alison Road is run down in appearance and often has vacant shops or is leased for low value retail tenancies. Historically the city block has accommodated a ‘value for money’ café/restaurant strip, dry cleaner, vehicular repair businesses and gymnasium.



**Figure 27: Avoca Street rundown appearance**

The urban design strategy addresses this issue through:

- A moderate increase in building height and density for the block bounded by Alison Road, Avoca Street, Rae Street and Lingard Street to incentivise redevelopment and upgrade of the heritage buildings, bringing additional residents to live in the centre and to support local businesses and renew the retail offering
- Including new public realm improvements in the planning for the centre to encourage the:
  - Upgrade of the pocket plaza associated with the historic Easts House at the northwest corner of the Alison Road and Avoca Street intersection
  - Upgrade of the Avoca Street footpaths with new paving and landscape treatments, including planting new canopy street trees where awnings do not preclude planting.

### Avoca Street – deterioration of heritage fabric

The urban design analysis has identified that many heritage listed and contributory buildings in RJTC were in a dilapidated state, had inappropriate paint colour schemes, infill windows of inappropriate design and detailing, and often retained disused signs or incorporated inappropriate new building signage.



Figure 28: Inappropriate paint finishes



Figure 29: Inappropriate infill window / glazing framing



Figure 30: Inappropriate signage mounting and location

The urban design analysis addresses this issue through:

- Strengthening Randwick DCP development controls that require new developments to address maintenance of heritage and contributory building shop frontages
- Incentivising the restoration and repainting of heritage buildings to their original colour scheme based on heritage advice provided by Council

### Avoca Street – dilapidated shopfronts

There are sections of the town centre that are particularly run down and dilapidated, such as along Avoca Street.



Figure 31: Dilapidated shopfronts and heritage facades on Avoca Street

The urban design analysis addresses this issue by proposing:

- A moderate increase in building height and density for key blocks in the town centre, to stimulate renewal



### Poor pedestrian experience and lack of quality public places

Many of the urban places within RJTC are poorly designed and unwelcoming for pedestrians to visit and to use. There is a lack of street tree planting, inappropriate street tree selection, excessive road space/carriageway widths (certain locations), and a lack of pedestrian prioritisation.

Further, in many locations the footpaths are in poor condition, and the power supply is by overhead wires with timber poles and cross arms. The street furniture could provide a more coherent identity for the town centre and there is a lack of public artwork, particularly communicating aboriginal history and perspectives to enliven the pedestrian experience.



**Figure 32: Poor interface between shopping centre and June Moore Place**

The urban design analysis addresses this issue through:

- Planning for a prioritised upgrade of the public domain of the town centre
- Identifying the key issues and where there are opportunities to improve the town centre public domain
- Ensuring new development uplift contributes to the funding of the upgrade of the public domain through contributions

### Night time economy

The night time economy is poorly catered for in RJTC. The traditional after hours zone is in the northeast of the town centre, along Avoca Street and Belmore Road, with the Coach and Horses Hotel the main hub of activity.



**Figure 33: Dilapidated state of the traditional night time zone**

The urban design strategy addresses this issue through:

- Planning for a renewed night time hub and restaurant strip along Avoca Street and Belmore Road, with renewal encouraged by moderate increases in height and density to stimulate the redevelopment of properties and the updating of restaurants, bars, small gallery and night time venues to supplement the existing traditional hotel activity.

## Flood management

Three flood catchment areas and their associated flood studies intersect in the north of the town centre. Figures 34 and 35 are composite maps illustrating the respective modelled hydraulic hazard level of a 1% AEP and a PMF flood event.

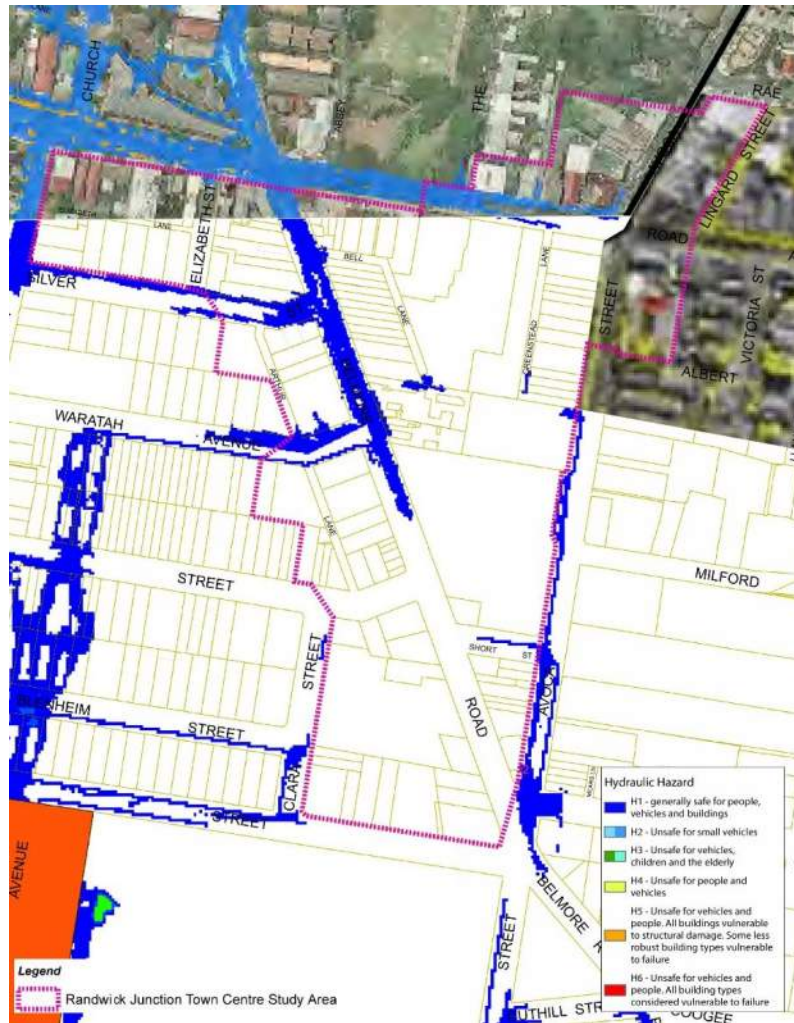


Figure 34: Hydraulic hazards level 1% AEP map

Development controls in the Randwick LEP and DCP will need to address the Ministers Direction 4.1 Flooding, including provisions for flood affected properties up to a PMF, such as minimum ground floor level for habitable spaces and for basement driveway thresholds, managing floodways along streets and when they traverse properties and provisions to ensure safe evacuation.

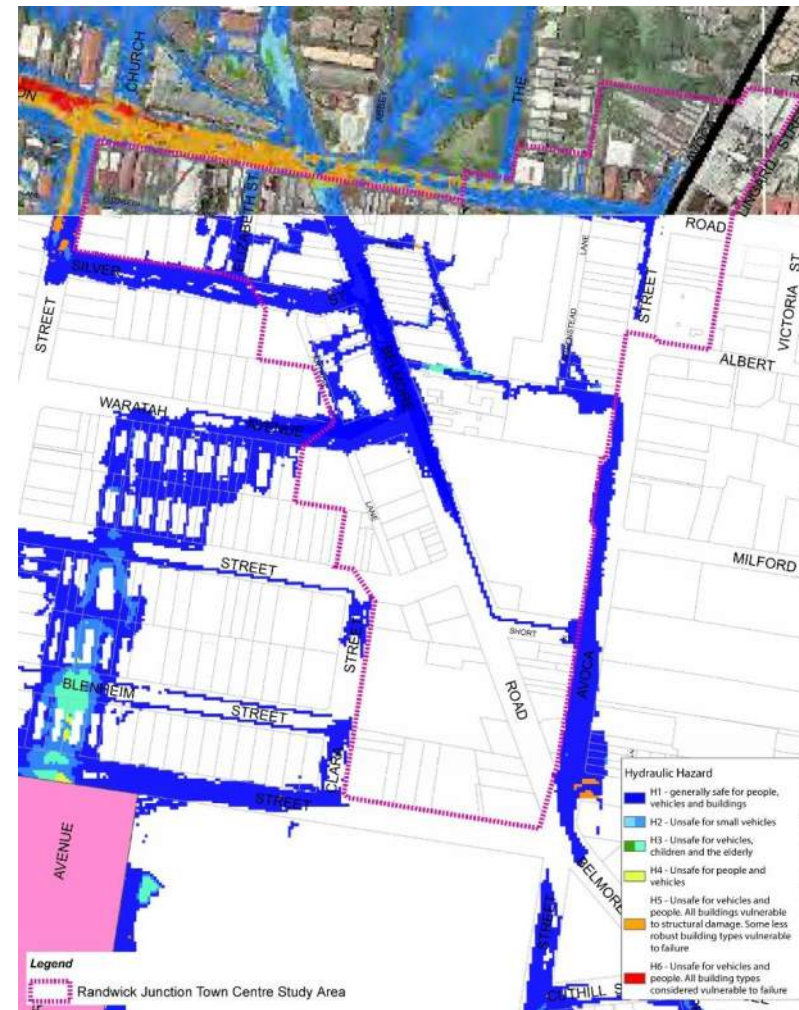


Figure 35: Hydraulic hazards level PMF event map

## Local Planning Directions 4.1 Flooding

Local planning directions 'Focus Area 4: Resilience and Hazards' includes section 4.1 Flooding that came into effect on 20 February 2023. RJTC is impacted by both 1% AEP and PMF flooding events.

Minister's Direction 4.1(3) refers to the requirements for Planning Proposals regarding property within the Flood Planning Area (FPA) i.e. 1% AEP flood event. Direction 4.1 (4) establishes the planning requirements of a flood event between the FPA and the more extensive PMF.

### Direction 4.1 (3) – FPA

Clause (3) (a) specifies that planning proposals must not allow for development in flood planning areas. Similarly, Clause (3)(c) specifies that planning proposals must not allow for residential development in High Hazard Areas (defined as hazard levels from H3 to H6). Table 1 below provides an assessment of the flood impacts for RJTC in respect of the key clauses of the Minister's Direction.

**Table 1 – RJTC Flood Assessment – Direction 4.1 Section (3) – FPA (1% AEP Event)**

<b>Direction 4.1 (3)(a) – is property in the FPA (1% AEP Event) impacted by a floodway?</b>	<b>Direction 4.1 (3)(c) – is residential property in the FPA (1% AEP Event) impacted by an area of high hazard? (H3 to H6)</b>	<b>Direction 4.1 (3)(e) – can higher risk accommodation in the FPA (1% AEP Event) effectively evacuate?</b>
Floodway areas are confined to the roads. There are a few properties that overlap remaining flood function categories (flood fringe or flood storage) as follows: - 4 Elizabeth Street, 20, 22 – 24 and 35 – 43 Belmore Road and 162 and 174 – 178 Avoca Street contain patches of flood fringe area. - 13 – 15 Silver Street, 48 – 60, and 57 – 63 Belmore Road, and 2 Albert Street and 73 – 109 Belmore Road contain flood fringe areas and patches of flood storage area.	In the 1% AEP event, flooding is generally confined within the existing roadways and flood depth does not exceed 0.15 m. There are a few properties with 1% AEP affectation, but depths within these property boundaries are also typically below 0.15m. High depths of flooding are present at 2 Albert Street (155 Avoca Street), 57 – 63 Belmore Road and 73 – 109 Belmore Road Within these properties, there are highly localised areas of 1% AEP flood with a depth greater than 0.15 m and up to 1.0m. It is noted that	In the PMF event the majority of the properties included in the RJTC Planning Proposal are flood free or fall within the lower H1 and H2 categories of flood hazard. Roadways across the RJTC primarily have a H1 flood classification and therefore if access to or egress from RJTC was required it would likely be available for all events up to and including the PMF via rising road access. It is noted that higher hazards are present along Alison Road at the northern boundary of the Site; however, egress from and access to these properties would be available via Elizabeth Street via low hazard routes.

	these flooded areas with up to 1 m flood depth are not connected to / part of any overland flow path: they are isolated flood areas that have been formed mainly due to the existing corridors between buildings or open spaces trapped within the affected properties' boundaries. In the 1% AEP event, flood hazard on the roads and within the affected properties is H1, except for those isolated areas of higher flood depths described above where hazards of up to H3 are present.	
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### Direction 4.1 (4) – FPA to PMF to which Special Flood Considerations Apply

Clause (4) applies to properties between the FPA and the PMF to which Special Flood Considerations apply. The zoning of Randwick Junction Town Centre is either R3 or E2 and permits sensitive development including Boarding Houses, Child Care Centres, Group Homes, Hostels, Respite Day Care Centres and Seniors Housing, thereby triggering the requirements of this clause.

Clause (4) (a) prevents provisions for development in floodway areas and Clause (4)(e) requires the safe occupation of (Shelter in Place) and efficient evacuation of a property. Table 2 below provides an assessment of flood impacts for RJTC in respect of the key clauses of the Minister's Direction.



**Table 2 – RJTC Flood Assessment - Direction 4.1 Section (4) – FPA to PMF and Special Flood Considerations**

Direction 4.1 (4)(a) – is property between the FPA and PMF impacted by a floodway?	Direction 4.1 (4)(d) – can higher risk accommodation located between FPA and PMF effectively evacuate?	Direction 4.1 (4)(e) – can property between the FPA and PMF ‘shelter in place’ and effectively evacuate?
Consistent with Direction 4.1 (3) Floodway areas are confined to the roads. There are a few properties that overlap remaining flood function categories (flood fringe or flood storage) as follows: - 4 Elizabeth Street, 20, 22 – 24 and 35 – 43 Belmore Road and 162 and 174 – 178 Avoca Street contain patches of flood fringe area. - 13 – 15 Silver Street, 48 – 60, and 57 – 63 Belmore Road, and 2 Albert Street and 73 – 109 Belmore Road contain flood fringe areas and patches of flood storage area.	In the PMF event the majority of the properties included in the RJTC Planning Proposal are flood free or fall within the lower H1 and H2 categories of flood hazard. Roadways across the RJTC primarily have a H1 flood classification and therefore if access to or egress from RJTC was required it would likely be available for all events up to and including the PMF via rising road access. Figure 5.1 shows proposed egress routes to flood free land in the vicinity of the RJTC. It is noted that higher hazards are present along Alison Road at the northern boundary of the Site; however, egress from and access to these properties would be available via Elizabeth Street via low hazard routes.	The shelter-in-place guidelines for flash flooding (DPE, 2025) provide several considerations for consent authorities in determining an appropriate emergency management strategy for a Site. The guidelines indicate that where evacuation off-site is achievable, shelter-in-place is not considered an appropriate emergency management strategy. In the case of the RJTC, off-site evacuation is available via a low-hazard egress routes throughout the study area. However, it is also noted that the flash flooding nature of the catchment and the low hazard flooding within the wider RJTC for all events up to the PMF may indicate that flood durations would be very short and egress into even low hazard floodwaters in such events would expose occupants to greater risk than simply “staying put”. Further analysis of potential durations of flooding is recommended as part of future development works. In the case of the RJTC, off-site evacuation is available via a low-hazard egress routes throughout the study

		area. However, it is also noted that the flash flooding nature of the catchment and the low hazard flooding within the wider RJTC for all events up to the PMF may indicate that flood durations would be very short and egress into even low hazard floodwaters in such events would expose occupants to greater risk than simply “staying put”. Further analysis of potential durations of flooding is recommended as part of future development works.
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## 4.7. Constraints and opportunities

### 4.7.1. Constraints

#### Heritage and contributory buildings

Heritage and contributory buildings should be considered as both a constraint and an opportunity. The majority of the town centre is within the Randwick Junction Heritage Conservation Area (HCA) and there are a significant number of heritage listed buildings and contributory buildings that should be retained, conserved and restored as part of any new redevelopment.

The distinct and ‘fine grained’ village character of the town centre is largely established by the heritage and contributory buildings. The streetscape character is valued by the community and therefore should be considered an asset (or opportunity) for any business.

Whilst the conservation of heritage and contributory building fabric may limit some redevelopment opportunities, the benefits gained by retaining and restoring heritage buildings comes through the unique character they embody and contribution to the public domain and streetscape.

The RJTC Planning Proposal seeks to strengthen the heritage character and values of the town centre and adopt a fine grain approach to the growth and development of the town centre.



**Figure 36:** Heritage and contributory items in the RJTC

### Recent development

Recently completed buildings (in the last 5-10yrs) in the RJTC are likely to remain for many years into the future and pose a constraint on future redevelopment and site consolidation.



**Figure 38:** Rear of 150 Avoca Street, Randwick



**Figure 37:** 12-14 Belmore Road, Randwick



**Figure 39:** 155-161 Avoca Street, Randwick – an example of recent development

### Major strata buildings

Residential and commercial buildings in the town centre that are in strata ownership are a potential constraint on the consolidation of sites for future redevelopment. Shop top development in the town centre is generally in strata ownership and the Randwick Plaza Shopping Centre is in commercial strata ownership.

Minor strata buildings (i.e. strata buildings with less than 10 strata units), are generally considered less constrained and could potentially be redeveloped in the short-medium term. Larger strata titles could potentially constrain future development opportunities.

### Existing subdivision pattern

Much of the town centre is characterised by a fine-grained subdivision pattern. Exceptions include the shopping centre properties and other consolidated land parcels, such as the Strategic Sites. The multiple ownership pattern may constrain and delay the potential for redevelopment.



**Figure 40:** Narrow allotments along Belmore Road

### Street tree planting and landscaping

There are several potential constraints to street tree planting in the town centre. Existing afternoon peak hour traffic volumes, southbound on Belmore Road is near peak capacity. This necessitates the retention of the current peak hour no parking requirement for the east, kerb side lane of Belmore Road.

Footpath widening into the kerbside lanes of Belmore Road is further constrained by the many bus only zones along the west and east sides of the road. These public transport requirements unfortunately constrain the potential for street tree planting and footpath widening along Belmore Road – there are limited opportunities along the west side.

Further, Alison Road and Avoca Street are constrained by their State Road status, identifying them as primary vehicular arteries under the control of the State Government.





Figure 41: Randwick Junction existing public transportation routes

#### 4.7.2. Opportunities

##### Renewal and restoration of heritage and contributory buildings

There are opportunities to restore heritage and contributory buildings in the town centre and thereby progressively enhance the quality and character of the town centre's streetscapes. A specific opportunity is the restoration of a row of two storey Victorian houses along Alison Road that form part of the original Brisbane Estate and the proposed new heritage item 'Montrose' at 179-181 Alison Road.

The houses remain hidden behind 1960s and 1970s commercial additions and modifications that detract from the Alison Road streetscape and the Randwick Junction Heritage Conservation Area. The street setback (former front gardens), if reinstated, could be an outdoor dining opportunity overlooking Alison Park. The restored houses could accommodate residential foyer and café/restaurant uses on both levels or incorporate an apartment above.



**Figure 42:** Victorian houses (orange) restoration opportunity

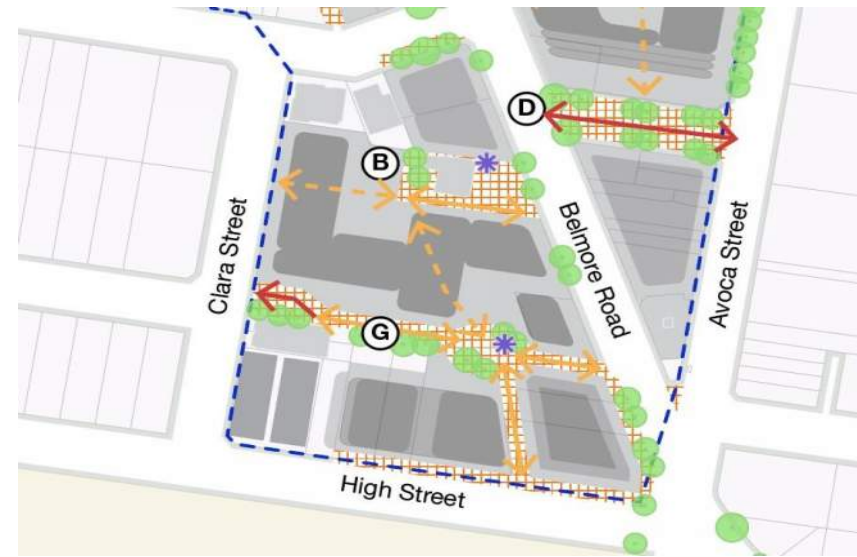
##### Renewal of northeast RJTC

The existing heritage fabric along Avoca Street in the northeast of the town centre is currently in need of renewal and revitalisation. There is an opportunity to encourage the redevelopment and upgrade of these heritage properties (particularly ground floor uses) to contribute life and activity to this part of the town centre. This part of the town centre is also important as a night time destination and would benefit from renewal and activation.

##### Create mid-block pedestrian connections

The study identified opportunities for additional through site and mid-block links to enhance east-west connectivity and general overall permeability within the town centre.

The larger Strategic Sites, such as the two shopping centres, provide opportunities for additional pedestrian links, as open air or as arcades/malls.



**Figure 43:** Opportunities for east-west through-site links

In addition to the mid-block links, there is the opportunity to link Sandgate House with Blenheim House in a cultural/community sense and in terms of convenience of pedestrian access between them. An improved pedestrian connection would facilitate joint arts and cultural events utilising both facilities and there is the potential to activate June Moore Place as an outdoor venue for the display of public art on the main street of the town centre. Such activities would be in addition to the existing use by Randwick and District Historical Society's museum and archives collection and role as a Heritage Centre for the municipality.

##### Identify locations for footpath widening

Increased footpath widths in key locations within the town centre can provide improved capacity for pedestrian flow, outdoor dining opportunities, with the additional setback of new buildings, reduced overshadowing and building bulk and provide sites for public art installations. Opportunities to consider widening include along Belmore Road west side and at the two shopping centres, at High Street adjacent to the Light Rail stop to accommodate increasing pedestrian movements, opposite Alison Park on Alison Road to create additional outdoor dining opportunities and along Alison Road (west) to provide additional footpath width for pedestrians and street tree planting.



### **Encourage health and innovative spaces along High Street**

New height and density controls along High Street provide an opportunity to leverage the proximity to the Randwick Education and Health Precinct (Randwick Hospital and the UNSW) and the Randwick Light Rail stop. Ground Floor and podium level health, medical and innovative start-up businesses should be encouraged. Increased employment and health and innovation spaces and a focus on retail, local services, medical and community uses, contributes to the role of RJTC in supporting the growth and liveability of the overall Randwick Collaboration Area.

### **Renewal and revitalisation of the public domain**

The Planning Proposal provides an opportunity for the renewal of the public realm, and the upgrade of the network of places within the town centre. The current state of streetscapes and plazas in the town centre is often dilapidated and uninviting, lacking a clear program of use, resulting in them being underutilised. There is an opportunity to provide new shared/pedestrianised zones, widened footpaths, new public places, street tree planting, landscaping, outdoor alfresco and dining areas, upgraded street furniture and public artwork. Specifically, there is the opportunity to increase the visibility of Randwick's aboriginal history through artwork and collaborative place design.

### **Introduce new drop-off / pick-up locations**

With the wide adoption of ride share services, the introduction of new drop-off / pick-up zones in RJTC could assist with increasing the accessibility for vehicle passengers. The introduction of a short stay bay as part of the upgrade of Short Street to a shared zone can accommodate retail related drop offs/pick ups, while a new short stay zone along Clara Street could facilitate accessibility for trips to access health services within and adjacent to RJTC. As part of the Clara Street zone there is the opportunity to improve the amenity (visual, weather protection and passive surveillance) of the existing bus stop on the east side of Clara Street through site redevelopment and streetscape works.

### **Randwick library street address**

If Royal Randwick Shopping Centre, which houses Randwick Library, was to redevelop in the future this could open up an opportunity to improve accessibility by incorporating a public street address/entry. In the longer term, investigations could be made to establish the optimum location for the library within the town centre. Alternative options might include a site with a direct street frontage or to be better integrated with existing Council owned facilities such as Sandgate House. Potential locations include adjacent to, or incorporating Sandgate House, areas fronting Short Street (which is proposed to be transformed into a shared zone) through to providing activation of newly created pedestrian links.

### **Opportunities for laneway activation**

The study identified the opportunity for laneway activation. Arthur Lane and Bell Lane have the potential for activation and prioritising for pedestrian access. Laneway activation provides opportunities for small scale commercial and retail uses benefiting from the increased pedestrian activity within the town centre to provide high quality and interesting spaces thereby expanding and making more interesting the pedestrian network.





## 5. Urban design vision and principles

### 5.1. Urban design vision

The urban design vision for RJTC captures a people focussed experience of the public realm – well scaled streets, plazas and urban parks.

Council's strategic outlook is for a high-quality urban environment, respect for heritage, a town centre with ready access to public transport, an environment that is easy to walk and to cycle, and that performs well environmentally, with good tree canopy cover and a sustainable approach to the design of buildings and the components of the public domain. Furthermore, Randwick Junction's success will depend on its transformation as a place that is functional, modern and that inspires, that recognises our Aboriginal heritage, and that incorporates quality street furniture and inspired public art.

The urban design vision for RJTC is for:

*'A lively and pedestrian friendly town centre, with a series of interconnected public places and attractive streetscapes that celebrate Randwick Junction's unique cultural heritage, that strengthens its 'village character' and that looks to the future, embracing a viable and environmentally sustainable ethos in the design of the buildings and the public domain.'*



## 5.2. Urban design principles

The following urban design principles developed for the RJTC Strategy in 2020 remain relevant and have informed the development of the preferred urban design approach for the town centre:

- To protect and enhance the village scale streets and the heritage significance of the area by promoting excellence in architecture, which positively contributes to the public realm
- To provide great public places – rejuvenated public squares and activated laneways to provide a variety of high quality, interesting spaces within the town centre into the future
- To promote commercial and retail growth with a key focus on select opportunity sites to meet the expected future demand generated by the adjoining education, health and innovation precincts
- To promote a balanced approach to transport and traffic that provides ease of access to sustainable modes of transport
- Encourage a mix of business, which caters to the local and broader community
- Preserve and enhance the area’s distinctive heritage identity and sense of place
- Focus density and taller buildings in a limited number of major strategic sites with large floor plates and potential to deliver improved public domain and urban design outcomes
- Strengthen the consistency of building frontage heights in the streets of the town centre to provide a well scaled pedestrian experience
- Widen the footpaths in specific areas of the town centre to accommodate pedestrian flow, new and existing street trees, alfresco dining, and at interfaces with transport stops
- Encourage opportunities for through-site/ mid-block pedestrian links, internal courtyards, and new or upgraded public places as part of any comprehensive redevelopment of major opportunity sites
- Encourage active frontages along main streets, continuing down side streets and laneways
- Seek opportunities to activate and extend retail/commercial activities to laneways

## 5.3. Community consultation

Community consultation was conducted to inform the development of the RJTC Strategy (RCC, 2020). Information gathering included a combination of in-house research, data collection and analysis and the commissioning of specialist studies and advice. Studies included a heritage review, land use audit, floor space and capacity analysis, urban design study, and built form and character analysis.

The preparation of the Issues Paper involved stakeholder engagement to understand local experiences, issues and aspirations with input from key stakeholders in the town centre, including businesses and institutions such as the Randwick Health Campus, UNSW and Marcellin College. Wider community consultation was also conducted through Council’s Your Say website and face-to-face engagement activities.

The outcomes from the stakeholder and community engagement process were incorporated into the Issues Paper and informed the preparation of the RJTC Strategy.



**Figure 44:** Community consultation stages

The RJTC Strategy community consultation received a total of 125 submissions from a broad range of stakeholders. The submissions were generally in support of the public domain improvements (landscaping and street trees), laneway activation and active transport (cycling, pedestrian connections) and opportunities for economic improvements outlined in the Strategy.

The draft Strategy received overall support from 64% of respondents, with the key reasons given, being the potential for public benefits, reinvigoration of the town centre, enhancement of the public domain, and the provision of affordable housing.

Several submissions raised concerns regarding potential traffic congestion, increased parking demand, concerns that uplift may adversely impact the existing built heritage, character and streetscape of the town centre.

Following Council's endorsement of the RJTC Strategy, there has been further one-to-one stakeholder meetings held with local property and business owners to discuss the RJTC Strategy, the planning objectives, and urban design approach.

Most owners of properties within the town centre are supportive of the RJTC Strategy of adopting site specific controls that seek to achieve a best practice urban design and heritage outcome. It is important that the centre continues to progressively update and renew itself, to stay relevant and serve a broad range of users into the future.

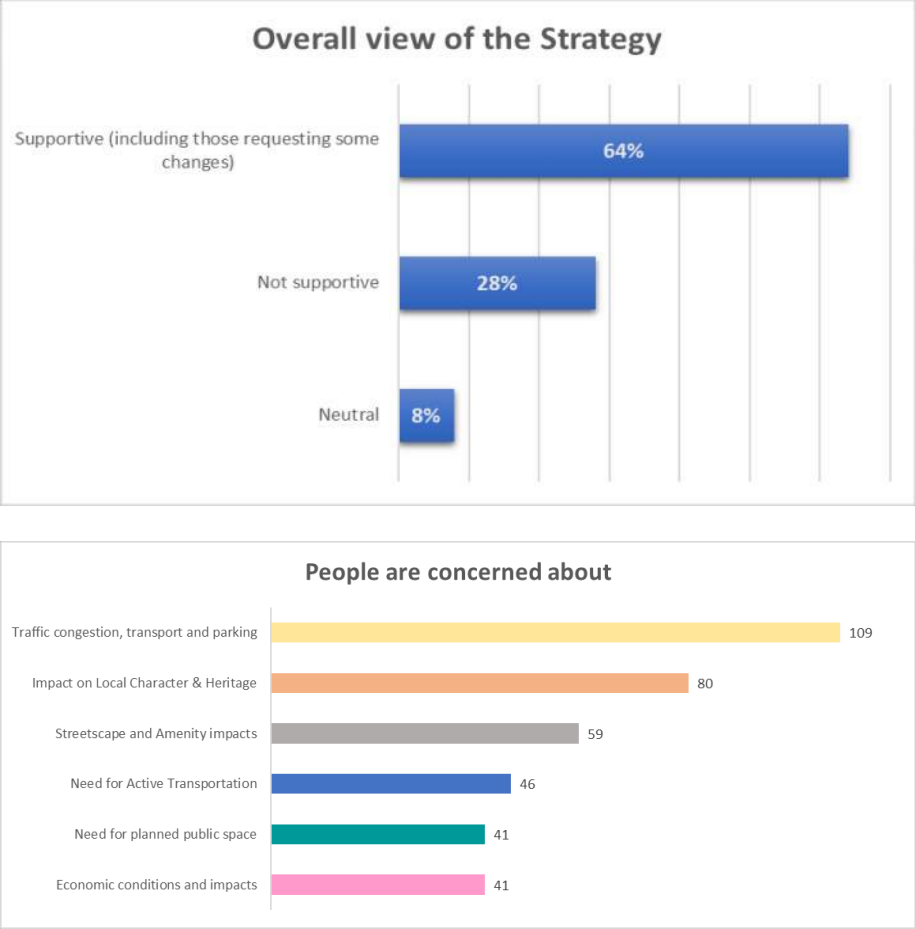


Figure 45: Key data from submissions



## 6. Urban design framework

### 6.1. Built form strategy

RJTC is currently characterised by predominantly two-to-three storey buildings, with some single storey development, two eight storey mid-rise towers (one commercial office, the other hotel/residential) and two shopping centres of two storey height with large floor-to-floor heights equivalent to three storeys in height (12m).

The proposed built form strategy is based on the following overall Height of Building (HOB) categories as shown in Figure 46:

- **Highly Valued Heritage Properties** – no change to the current controls is proposed – these sites retain their current zoning, FSR and HOB – generally 12m maximum height and 2:1 FSR
- **Heritage and Contributory Properties** – a modest uplift in height is proposed for development sites that include these items. Uplift of up to three storeys above the existing height limit (to a total of six storeys) is permitted, subject to a heritage assessment to determine the extent of significant building fabric to be retained – generally 5 or 6 storeys, or 18m to 21m height
- **Strategic Sites** – these sites are larger and less constrained by heritage considerations. An uplift in height is proposed to permit mid-rise residential tower and podium buildings – generally 11 to 13 storeys, or 36m to 45m
- **Gateway Strategic Site** – immediately adjoining the Randwick Light Rail station and marking the south gateway of the RJTC – the proposed mid-rise tower and podium building would be 40.5m, or 12 storeys.

Taller buildings are either located in proximity to the Randwick Light Rail stop, along High Street and at the corner of Belmore Road (the Gateway Site), or on large consolidated sites, not significantly constrained by heritage buildings, such as the shopping centre sites.

Generally a three storey street wall is established along the Belmore Road main street. Above the street wall, the building mass is set back to preserve the integrity of the heritage and contributory parapets and the overall scale of the street cross section. Where there are heritage considerations up to six storey buildings are permitted.

Where mid-rise towers are proposed they are set back from the Belmore Road main street to provide an uninterrupted street vista, maintain a suitable streetscape scale (and street cross section), and to avoid undue overshadowing of the retail main street.

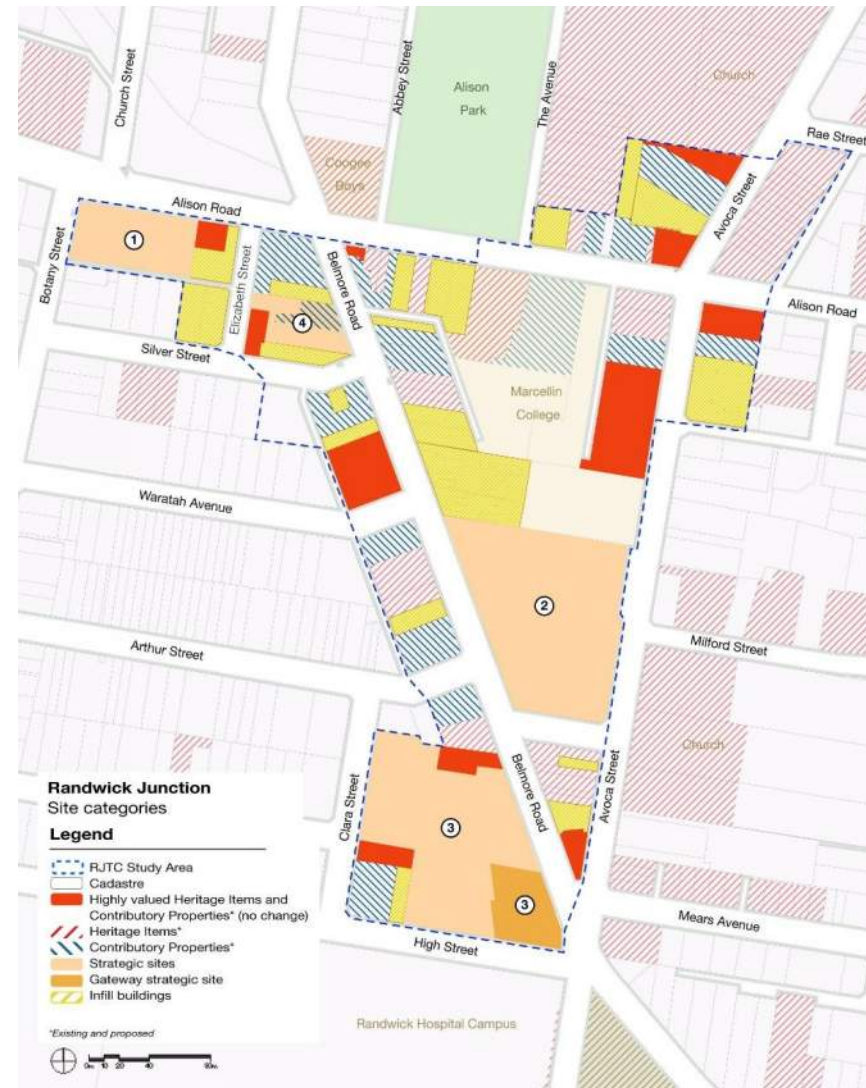


Figure 46: Site categories

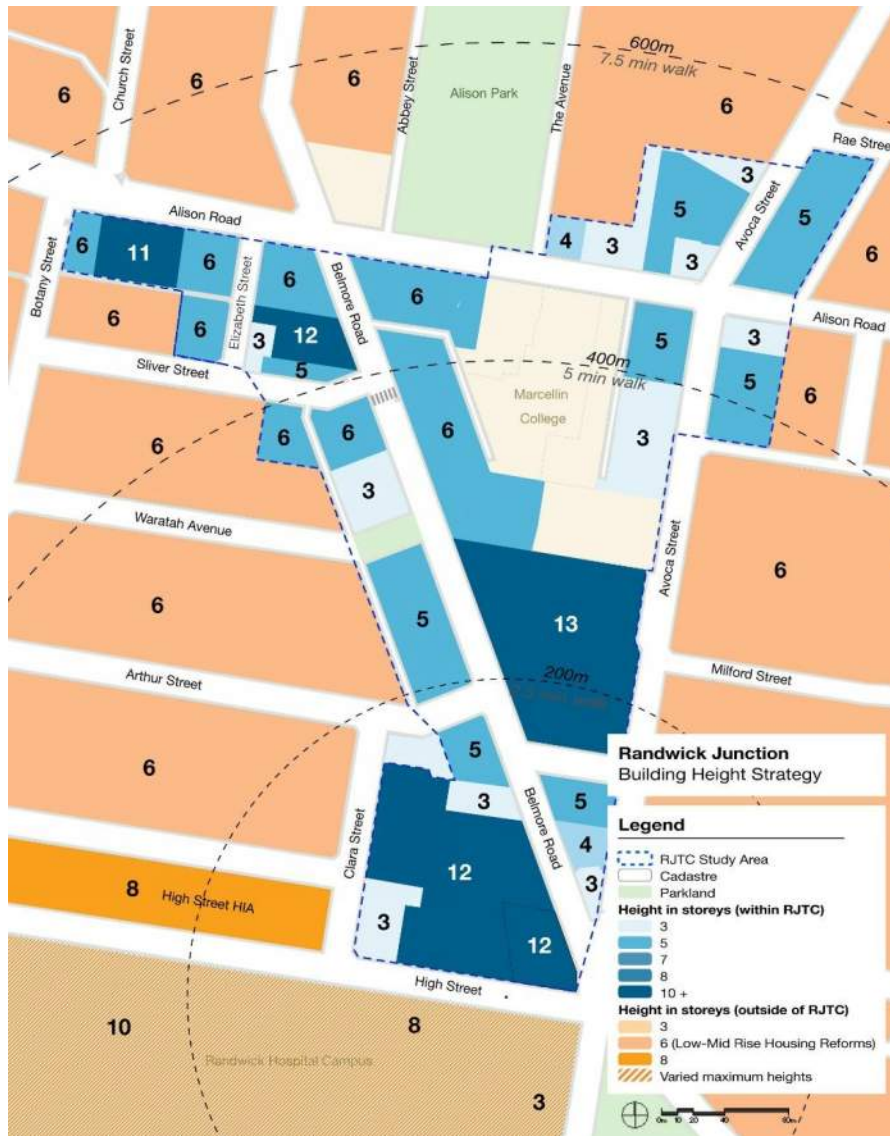


Figure 47: Proposed height strategy

Based on the above height categories, a site specific DCP will further refine and develop specific block-by-block building envelope development control plans that incorporate detailed street, podium and upper-level setbacks.

The proposed built form envelopes have been developed following detailed consideration of future planning and growth parameters, heritage and flooding considerations, pedestrian links, the individual site context, environmental constraints, the relationship with surrounding development and opportunities to enhance the public realm. These requirements will be set out in the RJTC site specific DCP that will be placed on public exhibition at the same time as the Randwick Junction Planning Proposal, for public feedback.

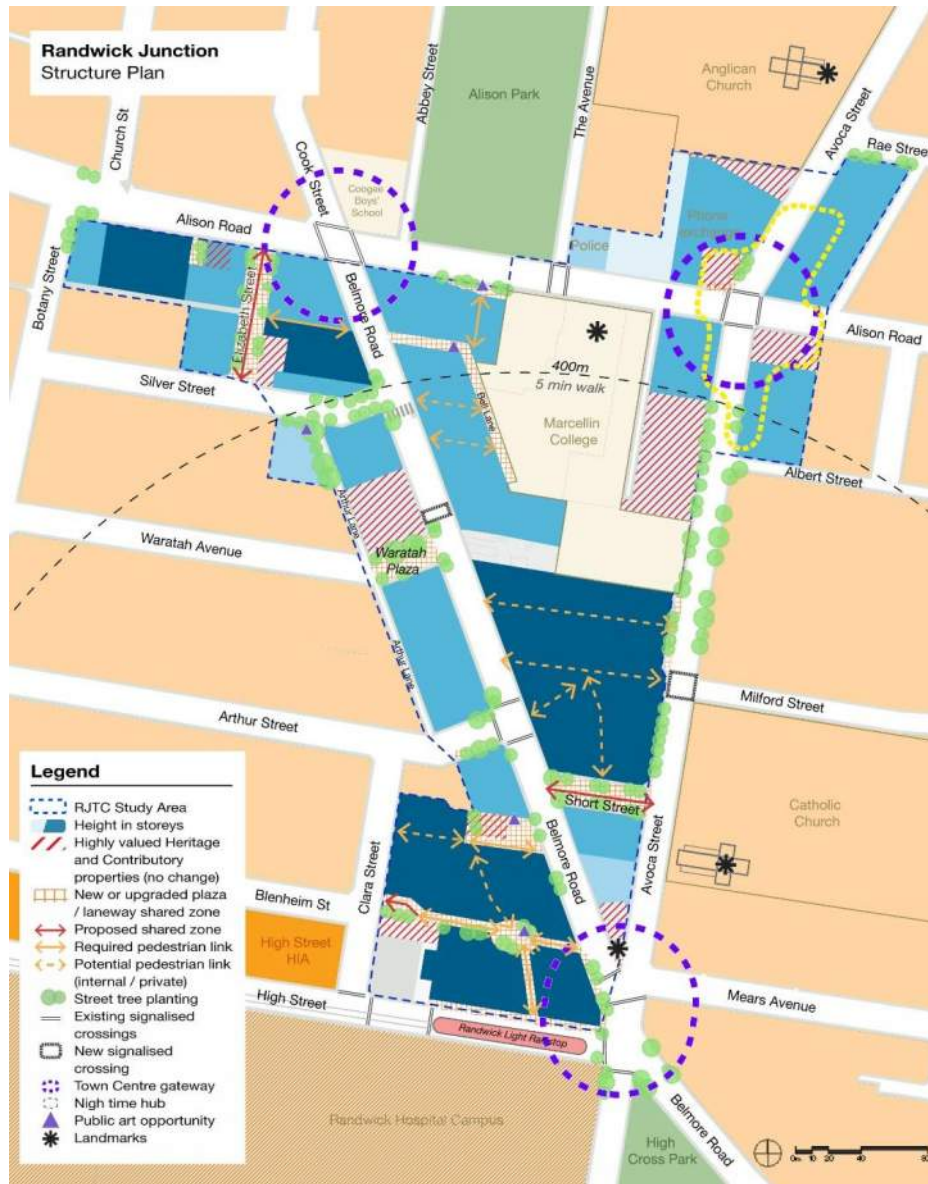


Figure 48: RJTC Structure Plan





Figure 49: 3D view northwest



Figure 51: 3D view southwest



Figure 50: 3D view west

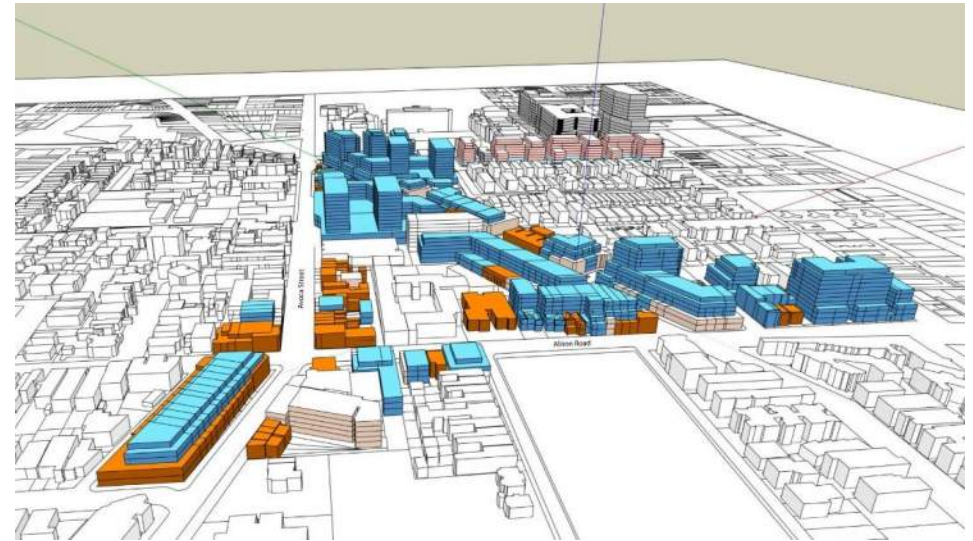


Figure 52: 3D view southeast



## 6.2. 3D visualisations

Detailed 3D modelling of RJTC has been undertaken by Council Officers to test various built form scenarios, to understand the town centre in totality to ensure a consistent approach is taken, and to analyse the proposed changes in height and density that would be experienced at eye level, and any potential impact on the scale and specific views to, and within, the town centre.

### 6.2.1. Captain Cook statue – triangle block

The castellated sandstone tower and Captain Cook statue at the intersection of Belmore Road, Avoca Street and High Street form a local historic and urban landmark for the town centre.

It is proposed that the planning controls (height and density) are unchanged for the existing Captain Cook statue (local heritage item I302) and 141-143 Belmore Road (local heritage item I302) identified in brown colouring in the 3D illustration below.

The existing two storey street wall along Belmore Road and Avoca Street is to be matched by any new construction, with an upper-level setback of minimum 4m required (determined by a site-specific heritage assessment of the extent of significant heritage built fabric). The profile of the heritage castellated tower against the sky is retained with upper-level setbacks and with a progressive stepping up to the north, from three, to four, to five storey maximum building height.



Figure 53: Proposed built form aerial 3D model



Figure 54: Existing - Corner of Belmore Road and Avoca Street, looking north



Figure 55: Proposed - Corner of Belmore Road and Avoca Street, looking north

### 6.2.2. Belmore Road / Alison Road corner

The three-storey corner heritage building at 1 Belmore Road will be preserved with its feature brick detail and lively roofscape of parapets, gable roof and party walls, as identified in brown colouring in the 3D illustration below. New construction will be required to be set back at the upper level and not touch the existing roofscape.

The existing predominantly two storey street wall along Alison Road and Belmore Road, that includes heritage buildings and contributory buildings, is to be matched in height by any new construction, with an upper-level setback of minimum 4m required (determined by a site-specific heritage assessment as to the extent of significant built fabric).

A further setback of the top level of any new building is required, and a 45-degree chamfer incorporated at the corner to reiterate the chamfer of the corner heritage building below.

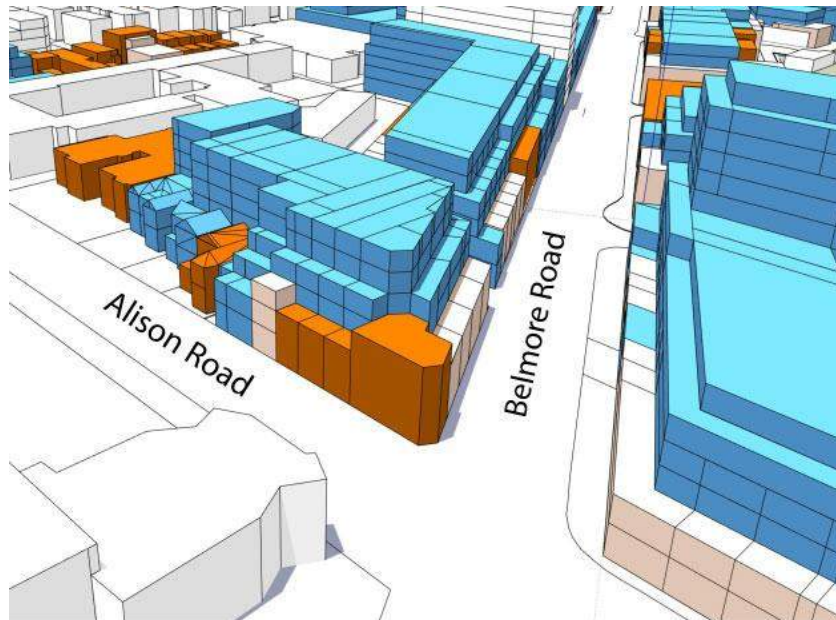


Figure 56: Proposed built form aerial 3D model



Figure 57: Existing corner of Belmore Road and Alison Road, looking south-east



Figure 58: Proposed built form at corner of Belmore Road and Alison Road



### 6.2.3. The Randwick Club Site

The heritage status and existing LEP controls for height and density (FSR) should remain unchanged for the two heritage terrace houses at 143 and 145 Alison Road (I255 and I256) indicated in brown in the 3D visualisation. The RLEP controls for the mixed-use development at 1 Elizabeth Street should also remain unchanged. Large deciduous street trees form an avenue along Alison Road and provide important shade canopy.

An overall eleven storey built form is proposed for The Randwick Club site, stepping down to a six-storey building at the corner of Alison Road and Botany Street, and to the south part of the block (along Elizabeth Street). A three-storey street wall is proposed along Alison Road and Botany Street. The buildings on the Alison Road frontage have a 2m building setback, and the other three streets a 1m setback, to allow for the widening of the footpath and the accommodation of street trees.

A pocket plaza of minimum 8m width, and of a total area to satisfy the ADG requirement for deep soil provision, is proposed between the terraces and the new building. It is recommended that Elizabeth Street be investigated as a one-way shared zone (prioritising pedestrian access) with new buildings incorporating ground floor level active uses. An upper-level setback of 4m, above the three-storey street wall is proposed, and a top level setback of 3m to reduce the apparent height of the building when viewed from street level.



Figure 59: Proposed built form 3D model



Figure 60: Current built form – Belmore Road looking south



Figure 61: Proposed built form – Belmore Road looking south



#### 6.2.4. Belmore Road

Belmore Road is the town centre's main shopping street and is currently defined by building frontages, varying in height from two to eight storeys. The predominant street wall height is two or three storeys determined by the height of existing heritage and contributory buildings in each block, identified in brown or tan colouring respectively, in the 3D illustration below.

It is proposed that the planning controls (height and density) are unchanged for four heritage items (35-43 (I294), 48-60 (I295), 128 (I301) and 141-145 (I302 and I303) along Belmore Road and 25 Waratah Avenue, as they are experienced and are significant 'in the round' therefore there is limited scope to incorporate additional height or extensions.

The existing predominantly two or three storey street wall along Belmore Road that includes heritage buildings and contributory buildings is to be matched in height by any new infill construction, with an upper-level setback of minimum 4m required (determined by a site-specific heritage study into the extent of significant built fabric).

A further setback of the top level of any new building is required where the street wall is two storeys, to produce a better scale relationship of the new upper-level building form with the existing two storey heritage or contributory building frontage.



Figure 62: Proposed built form - Belmore Road looking south-east



Figure 63: Current built form – Belmore Road looking south



Figure 64: Proposed built form – Belmore Road looking south

### 6.2.5. Belmore Road angled view

A quality urban design outcome is sought for a consistent and equitable built form for the properties along Belmore Road. Any new development must provide a well scaled and respectful approach to the heritage and contributory buildings of the streetscape, and that retains a level of variety and interest in the streetscape. Development may incorporate 'on merit' some variations and one-off architectural features within the overall urban planning controls.



Figure 65: Proposed built form aerial 3D model



Figure 66: Current built form – Belmore Road angled view



Figure 67: Proposed built form – Belmore Road angled view



### 6.2.6. Former CBA Site

This strategic site includes Contributory Buildings at 16-20 Belmore Road, indicated by the tan colouring in the 3D illustration, which are subject to a heritage assessment, to establish the extent of heritage building fabric to be retained to preserve their significance.

An overall 12 storey built form is proposed for the site, stepping down to a two and six storey podium and a three storey street wall, established by the existing two storey Contributory Buildings along Belmore Road, Alison Road, Elizabeth Street and Silver Street. The proposed three storey street wall at the corner mirrors the existing three storey street wall height on the adjoining corner to the south.

The three storey Residential Flat Building (RFB) on the corner of Elizabeth Street and Silver Street has existing street setbacks and new buildings on adjoining sites will be required to incorporate setbacks to maintain amenity - adequate access to natural light, cross ventilation and to provide privacy for residents.



Figure 68: Proposed built form aerial 3D model



Figure 69: Current built form – Belmore Road angled view



Figure 70: Proposed built form – Belmore Road angled view



### 6.2.7. Alison Road Brisbane Estate Victorian houses and alfresco dining

The southern side of Alison Road is characterised by a mix of restored Victorian houses (within the Marcellin College campus), several heritage and contributory buildings and three commercial detracting buildings added in the 1960s/1970s. It is proposed that the planning controls (height and density) are unchanged for the Marcellin College campus, including the two restored houses.

The predominant street wall height is two storeys along Alison Road. This height is to be matched by any new construction, with an upper-level setback of minimum 4m required (determined by a site-specific heritage study into the extent of significant built fabric) to produce an improved scale relationship of the new upper-level building with the existing two storey heritage or contributory building frontage.

There is an opportunity to remove the existing three detracting commercial frontages along Alison Road (no. 179-181, 135-185 and 187), to restore the two storey Victorian houses that sit behind (similar to the Marcellin College houses) and are mostly intact (evidenced by the hip/gable roofscape and decorative chimneys visible) by permitting new buildings setback at the rear of the properties to be developed to a mid-rise level (six storeys).

The setback produced along the Alison Road frontage would provide for a new alfresco dining destination and the night time activation of the north of the town centre with a new place with views overlooking Alison Park.



Figure 71: Proposed built form aerial 3D model



Figure 72: Current built form – Alison Road detracting commercial additions



Figure 73: Proposed built form – Alison Road restored estate houses

### 6.2.8. Randwick Junction Gateway Site

The consolidated properties at 160-168 Belmore Road are a designated Strategic Site (larger and less constrained than other sites in the town centre) located at the urban crossroads of Belmore Road, Avoca Street and High Street. The intersection forms an urban 'gateway' to RJTC and warrants special design attention and urban emphasis in the corner architectural expression to enhance the significance of the arrival at the town centre from the south.

To the east of the site is the heritage castellated landmark tower and Captain Cook statue, and to the south is a series of sandstone heritage buildings and a small plaza within the Ambulatory precinct of the Randwick Hospital campus along Avoca Street. To the southeast is the historic High Cross Park. The Light Rail Randwick stop is situated immediately to the south of the site, where High Street meets Belmore Road, and there is a bus stop to the northeast, on the west side of Belmore Road.

The Bright Alliance hospital building to the south of the site is eight storeys, and with its larger floor-to-floor heights, generates a building equivalent in height to a ten-storey mixed use building. The strategy is for the Gateway Site building to appear consistent with the height of The Bright Alliance building, and for the buildings to step down to the north to seven- eight storeys along Belmore Road. The High Street Well Located Housing Area (WLHA) extending from Clara Street to Botany Road located to the west of the site has a proposed height limit of eight storeys.

The consolidated site presents a unique opportunity to incorporate health and medical uses as well as other innovative and knowledge intensive uses which support the function of the strategic centre. This is also an opportunity for residential accommodation and public outcomes such as affordable housing, footpath widening and pedestrian links to be delivered. Any proposed uplift in this location should be tied to demonstrating design excellence as well as desired public outcomes.

The three podium levels of the Gateway Site building are envisaged to be commercial/retail uses, with medical/health related uses specifically encouraged, complementing the core hospital functions of Randwick Hospital. Such uses are anticipated to be accommodated in this development to meet non-residential FSR controls.

The overall building should be set back 3m from the existing street boundary, to provide more footpath space around the Light Rail station and for alfresco dining. The tower of the building is setback 4m above the podium level, has a chamfered corner and the top level of the building is setback a further 3m to reduce the apparent overall building height.

A network of new mid-block pedestrian links, and an oval shape plaza is proposed to open up the middle of the block and improve pedestrian access to the Light Rail stop.

In the long term, vehicular access to these High Street buildings, is proposed from a new mid-block laneway, to remove the multiple existing footpath crossovers currently present along the High Street and Belmore Road frontages. Footpath widening is possible at the Belmore Road and High Street corner, where the traffic flow geometry permits, and a no parking zone currently operates.



**Figure 74: Proposed built form aerial 3D model**





**Figure 75: Existing – Randwick Junction (High Street / Belmore Road) view**



**Figure 76: Proposed envelope – Gateway (High Street / Belmore Road) view**

### 6.3. Cross sections

A series of typical street cross sections were prepared to establish the appropriate scale for redevelopment, to test the requirements for overall building and upper-level setbacks in relation to the proportion of the street cross section, considering sightlines, and the predominant street wall established by existing heritage and contributory buildings.

The typical maximum building height for non-Strategic Sites is six storeys (21m), which is compliant with the Apartment Design Guide (ADG) floor-to-floor level standards for Ground Floor, First Floor and typical residential floor.

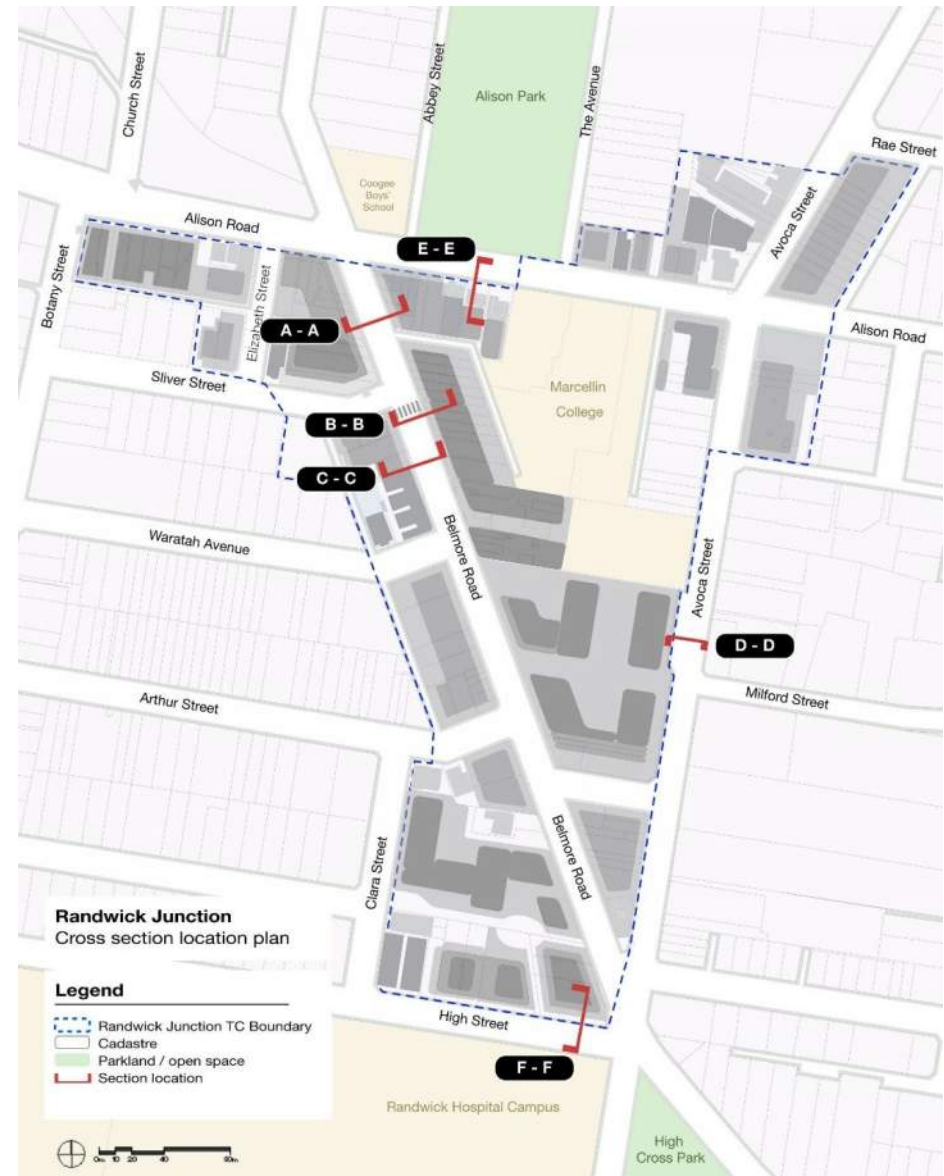


Figure 77: Cross section location plan

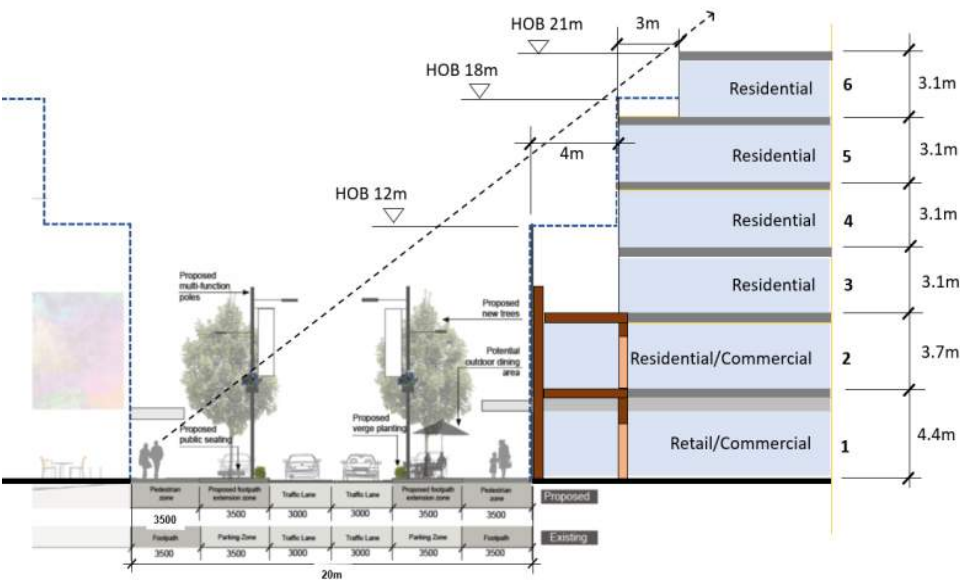
**Two Storey Heritage or Contributory Building (A-A)**

An initial upper-level setback of minimum 4m is required to any new development above the height of the existing street wall (generally two or three storeys). Where the street wall is two storeys as shown in Figure 78, a further top level setback of 3m is required to ensure a good proportion is achieved of two, two and two storey respectively.

The extent of significant heritage fabric of existing main street heritage and contributory buildings is to be established by specialist heritage architectural advice at the DA stage to inform the extent of retention necessary to maintain the integrity of the heritage or contributory building.

At least the first full room of a heritage or contributory building should be retained. If new construction is deemed appropriate at the rear of the site, new floor levels should align seamlessly with the existing building floor levels and the interface of the new and existing architecture considered and addressed in detail by the design proposal.

**Typical Street Cross Section  
Two Storey Heritage or Contributory Building  
6 storey - 21m max height**



**Figure 78: Typical Street cross section (two storey) heritage building**



### Three Storey Heritage or Contributory Building (B-B)

The typical maximum building height for non-Strategic Sites is six storeys (21m), compliant with Apartment Design Guide (ADG) minimum standards for Ground Floor, First Floor and typical residential, floor-to-floor levels.

As shown in Figure 79, an initial upper-level setback of minimum 4m is required to any new development above the height of the existing street wall (generally two or three storeys). The extent of significant heritage fabric of existing main street heritage and contributory buildings is to be established by specialist heritage architectural advice at the DA stage to inform the extent of retention necessary to maintain the integrity of the heritage or contributory building.

#### Typical Street Cross Section Three Storey Heritage or Contributory Building 6 storey - 21m max height

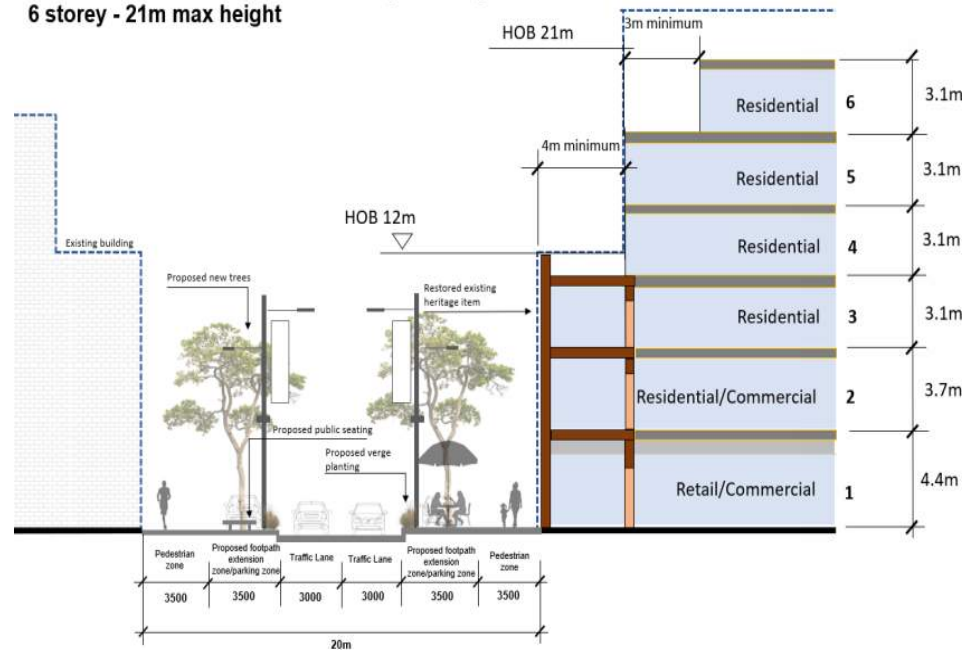


Figure 79: Typical street cross section (three storey)

At least the first full room of a heritage or contributory building should be retained. If new construction is deemed appropriate at the rear of the site new floor levels should align seamlessly with the existing building floor levels and the interface of the new and existing architecture considered and addressed in detail by the design proposal.

### Typical Main Street Infill Building (C-C)

The typical maximum building height for non-Strategic Sites is six storeys (21m), compliant with Apartment Design Guide (ADG) standards for Ground Floor, First Floor and typical residential floor-to-floor levels.

As shown in Figure 80 an initial upper-level setback of minimum 4m is required to any new development above the height of the existing street wall (generally two or three storeys). Infill buildings should match the predominant street wall height (two or three storeys) of the town centre block in which it occurs. The proposed street cross section establishes a well-defined urban streetscape of good scale and proportion, that has a consistent urban scale and built form character.

#### Typical Street Cross Section - Infill Building 6 storey - 21m max height

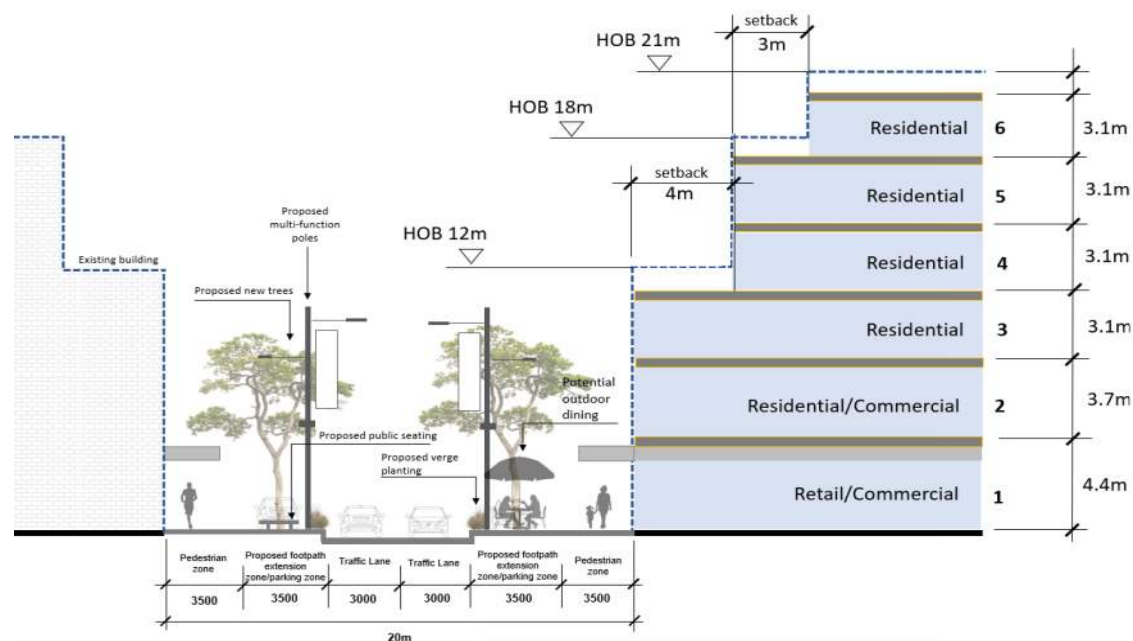


Figure 80: Typical street cross section (infill building)

### Typical Shopping Centre Development with Residential Above (D-D)

The two large scale shopping centre developments within the RJTC (Royal Randwick Shopping Centre and Randwick Plaza) have large floor-to-floor heights of about 5.5m. In this situation as shown in Figure 81, two large retail/commercial building levels (Ground Floor and First Floor) and an allowance for a podium level balustrade, establish a street wall height of about 12m - similar in height to a three-level infill, heritage, or contributory building in the town centre. This strategy will maintain a consistent street wall height along Belmore Road and the other streets in the town centre, whilst accommodating different building types.

The shopping centre sites are identified as Strategic Sites within RJTC, due to their larger size site areas and as they are less constrained than most sites within the town centre. An opportunity for residential apartments above the podium level, around a central courtyard, has been identified. The maximum height proposed for the Royal Randwick Shopping Centre is 13 storeys. Mid-rise apartment buildings above the podium are setback 4m from the street wall along Belmore Road and Avoca Street; a minimum 11m setback required to the south (Short Street frontage) to maintain solar access to the Short Street public domain. The apartment buildings are clustered around a central garden on the roof of the podium that potentially may incorporate skylights for the central atrium of the shopping centre below.

In the situation where a commercial building is proposed above the two storey podium, the number of storeys that can be achieved within the maximum Height of Building (HOB) and setbacks (included within the Randwick Junction Town Centre DCP) may be reduced (due to the greater floor-to-floor heights associated with commercial uses)

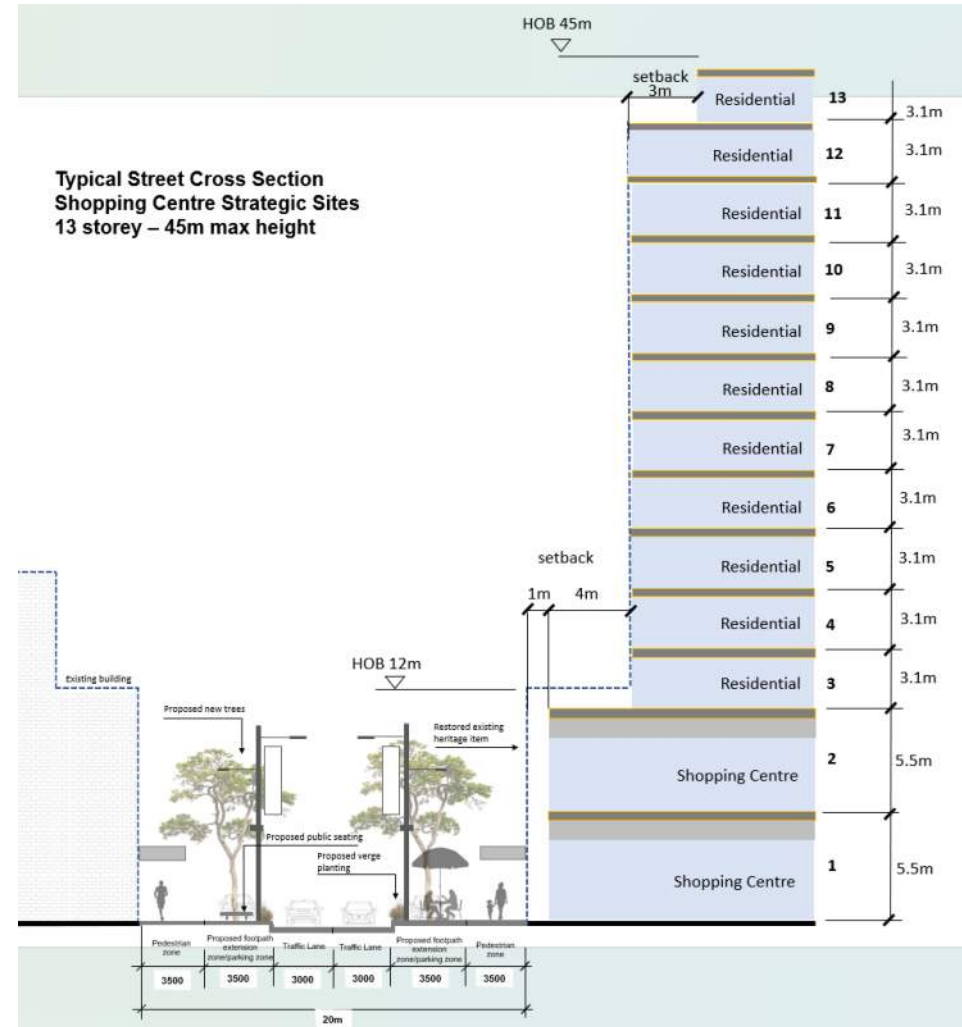


Figure 81: Typical street cross section (shopping centres)



## Alison Road – Restoration of Brisbane Estate Victorian Houses (E-E)

The Heritage Assessment, undertaken by City Plan Heritage, has identified an opportunity to restore two Victorian houses that form part of the original Brisbane Estate and the heritage item 'Montrose' at 179-181 Alison Road. These houses remain hidden behind 1960s and 1970s commercial additions that detract from the Alison Road streetscape and the Randwick Junction Heritage Conservation Area.

The top right aerial photo is from 1955 and illustrates the original streetscape and front garden setback of Glanmire house (1259) and Seabird house (1258) within the Marcellin College campus (in green outline) and the three houses at 179-181, 183-185 and 187 Alison Road (in orange outline). Any proposed uplift in this location should be tied to the removal of the commercial additions which detract from the heritage values of the town centre. The bottom right aerial photo of 2018 and site inspections, have established that much of the house fabric remains, including the original hip and gable roofscape and chimneys.

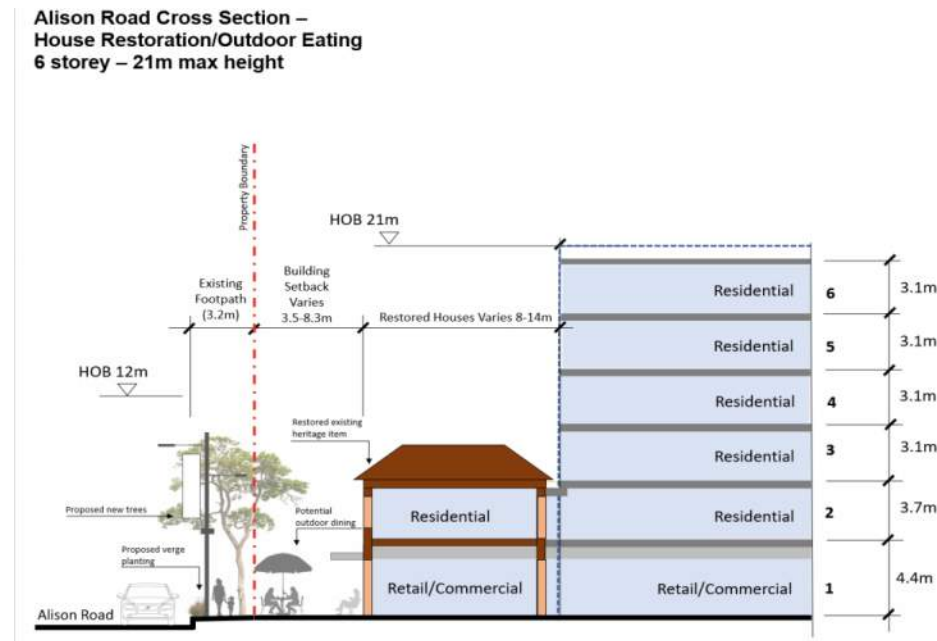


Figure 82: Typical street cross section (House restoration)

The street setback (former front gardens) are proposed as alfresco dining areas overlooking Alison Park. The restored houses could accommodate residential foyer and café/restaurant uses with an apartment above. Additional height is proposed at the rear of these properties (six stories) to compensate for the front setback and restoration of the Victorian houses. Testing of the pedestrian level sightline indicates that most of the proposed rear six storey building, would not be readily visible from the southern Alison Road footpath.



Figure 83: Sites identified for restoration (1955 and today)

## High Street – Gateway Building (F-F)

The sites at 160-168 Belmore Road are collectively identified as a Strategic Site (known as the Gateway Site) due to its immediate proximity to the Randwick Light Rail stop, being located across the road from the Randwick Hospital campus, and due to its prominent location at the crossroads at the south approach to RJTC.

A landmark corner mid-rise mixed-use tower building of 12 storey height (assuming a commercial podium and residential tower) is comparable in height with the recently completed The Bright Alliance building to the south of High Street and the Randwick Hospital campus. The new mixed-use tower would mark the important Belmore Road and High Street corner, establishing an urban gateway on the southern approach to the town centre. The building envelopes to the west of the gateway building remain consistent at 12 storeys. Mid-rise residential development is envisaged above the shopping centre podium of 5-12 storey in height.

As shown in Figure 84, the overall building is setback 3m from the property boundary, to widen the footpath, to accommodate additional pedestrian traffic and outdoor dining. A 4m setback above podium level is proposed to the mid-rise apartment tower, aligning with other areas of the town centre. A further top level setback of 3m reduces the apparent height of the building when viewed from surrounding vantage points.

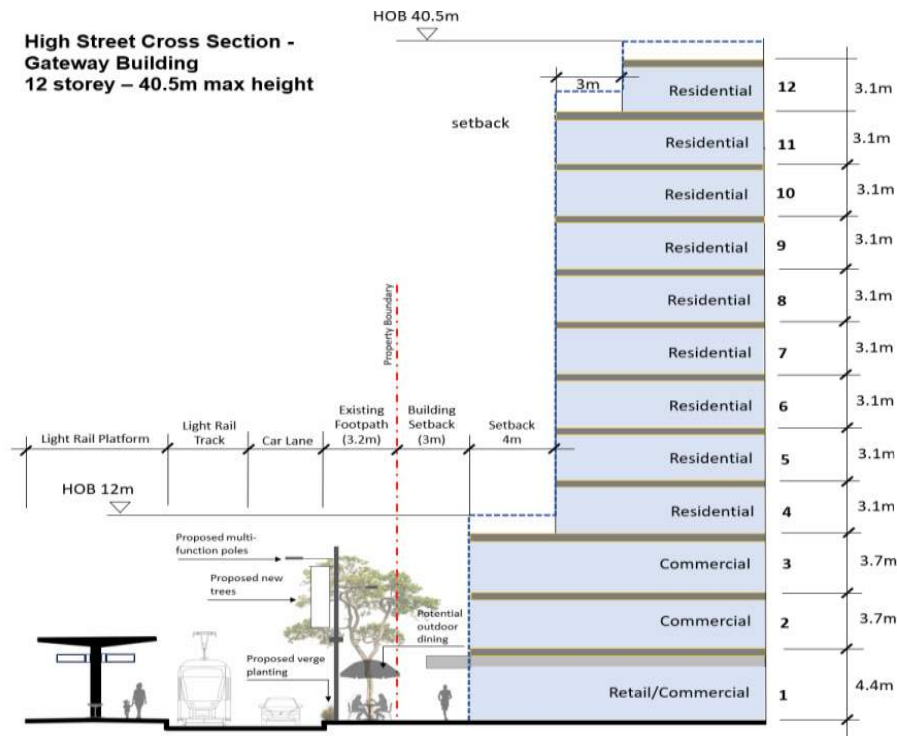


Figure 84: Typical street cross section (Gateway Building)

## 6.4. Affordable housing

### The need for affordable housing

The RJTC Affordable Housing Plan aims to ensure that lower income households continue to live and work locally within Randwick LGA, to facilitate a socially diverse and inclusive community, and to support the economic functions of the Randwick Education and Health Strategic Centre.

The Randwick City Affordable Housing Needs Analysis demonstrates the need to increase the supply of affordable rental housing in the Randwick Local Government Area (LGA). The Randwick Affordable Housing Strategy was adopted at the December 2025 Council Meeting. Without provision of more affordable forms of housing, the market will continue to produce more expensive housing. The flow on effect is that existing lower income groups would need to move out of the area, and new lower income households may be prevented from finding housing in the local area, close to employment and education opportunities within the Randwick Education and Health Strategic Centre.

The RJTC is close to frequent public transport services, local business centres and the Randwick Education and Health Strategic Centre. It is also within easy reach of the Sydney CBD, the eastern beaches and Centennial Parkland. The attractiveness of the area as a place to live and work will continue to place upward pressures on property values beyond the means of lower income households to purchase or rent in the area.

The RJTC Planning Strategy supports a new planning framework for the town centre to guide growth and change over the next 15 years. This new planning framework (via proposed amendments to Randwick LEP 2012 and DCP 2013) will allow for a higher intensity of development at appropriate locations and scale whilst also providing for community benefits including providing for the delivery of affordable housing dwellings for essential key workers.

### Objectives

The objectives of the draft affordable housing plan prepared for RJTC are to:

- Identify the need for affordable housing which will only be increased by renewal and redevelopment
- Recognise affordable housing as essential infrastructure necessary to support a socially diverse community and the economic functions of Randwick City
- Ensure there are opportunities for low to moderate income households who work or have family connections in Randwick City to live in Randwick City.

### Aims

The overall aim of the affordable housing plan for Randwick Junction is to deliver affordable housing for the community, utilising the contributions from redevelopment in the town centre. Over time, the plan will deliver approximately 62 affordable housing dwellings based on the total estimated dwelling capacity of the town centre.

### Affordable housing contributions

Figure 85 indicates the affordable housing contribution rate that is proposed to apply to land within the RJTC.



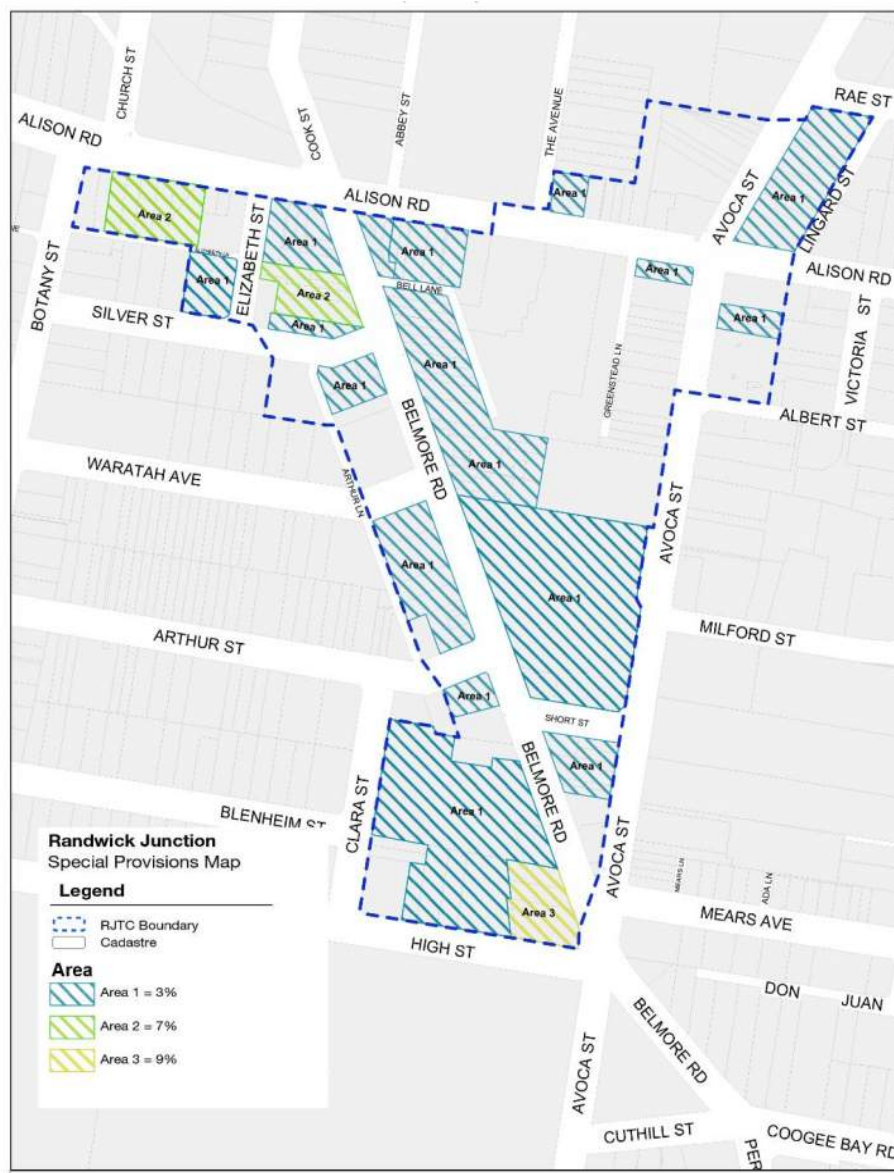


Figure 85: Affordable Housing Contributions Area map

The contribution rate is calculated on the total floor area intended to be used for residential purposes in all development applications on identified sites within RJTC, including the adaptive reuse of existing floorspace and new floorspace. The Affordable Housing Plan specifies exclusions to the payment of contributions.

In relation to the contribution for affordable housing, Council's preferred approach is by way of an in-kind dedication of completed units with any remainder being paid as a monetary contribution to Council. In this location, where land is premium and scarce, 'in-kind' contributions supply immediate affordable housing.

### Principles in establishing the affordable housing rates

The percentage levy applicable for properties in the RJTC is based on the detailed advice provided in the Randwick Junction Economic and Feasibility Analysis (SGS Economics and Planning, Final Report, 26 April 2023 & 2025).

The affordable housing rates are established based on the following principles:

- An affordable housing rate will only apply to those properties that have an increase in Floor Space Ratio (FSR) under the RJTC Planning Proposal
- The levy will only apply to residential land use (not commercial/retail uses) – this will incentivise the development of commercial land uses and employment in the town centre that will not be subject to a levy
- The remaining properties are based on a sliding scale percentage levy based on the amount of uplift in FSR for each property and feasibility, as follows:

FSR Uplift	Percentage Levy
Area 1	3%
Area 2	7%
Area 3	9%

An equitable approach is taken, with similar development types, such as the two shopping centres, having the same applicable percentage levy.

## 6.5. Public domain improvements

A high quality and attractive public realm is an integral component of an economically prosperous and socially vibrant town centre. The 'public realm' includes streets and laneways, footpaths, plazas, parks, street verges, car parks and other urban spaces. It also includes urban elements such as street trees and landscaping, paving, lighting, street furniture and public art.

It is envisaged that the development of sites within RJTC will contribute to the overall town centre by improving pedestrian connections, widening footpaths, providing pocket parks and plazas that provide areas of relief from the busier streets and footpaths of the town centre.

The existing public domain within RJTC provides opportunities for revitalisation and uplift, particularly within the town centre's existing public space and plazas that will contribute to providing a greater 'sense of place' and improved liveability.

The following conceptual designs envisage potential improvements to the public domain, including improved site layouts and designs of existing public spaces, landscaping treatments and upgraded facilities to improve the aesthetic appearance and attractiveness of public space within the town centre.



Figure 86: Public Improvements map



### 6.5.1. June Moore Place (A)

The existing public space known as June Moore Place has the potential to be reconfigured into a public plaza with a seamless transition between adjoining commercial uses and the plaza. The interface with the Randwick Plaza Shopping Centre could be improved with a café and seating activating the place.

High quality landscaping of the public plaza would enhance and soften the existing qualities of the place and integrate it more effectively with the Belmore Road footpaths. Integrated paving, seating and landscaping would strengthen the setting for the State listed heritage item 'Sandgate' house and allow the community to better appreciate and interact with the space. There is also opportunity to revitalise Sandgate house and June Moore Place as a potential community/cultural use such as artists studios in line with the Randwick Arts & Culture Strategy Direction to 'create an award winning nationally and locally recognised cultural arts precinct in the Randwick Junction Town Centre by 2031'.



Figure 87: Concept perspective of June Moore Place

### 6.5.2. Easts House (B)

Easts House (formally the Randwick Post Office) has the potential to provide a quality public space and offer pedestrians a small urban refuge along Avoca Street. Part of the space is situated on privately owned land. Consultation and collaboration between Council and the property owners would facilitate the implementation of improvements of the public plaza.

The plaza concept design proposes new landscaping and planted buffers along the edge of Avoca Street providing a natural barrier between pedestrians and traffic. The proposal includes new seating, feature trees and planting that would renew an existing underutilised space in the northeast of the town centre.



Figure 88: Concept perspective of East House Corner



### 6.5.3. Short Street (C)

Short Street has the potential to be transformed into a pedestrian prioritised shared street (retaining the current single lane one-way vehicular access) to create a new public space for people to meet, dwell and socialise in the south of the RJTC. Activation of the north and south building edges of the pedestrianised place would be essential to its success.

With an ideal northern aspect (due to the east-west layout), the south side of Short Street would suit alfresco dining and public seating benefitting from good solar access. As illustrated below, some parallel car parking can be retained and integrated into the shared space with planters and landscaped barriers demarcating car parking areas. Such design matters should be considered in the RDCP review.

The scheduling of any upgrades to Short Street should be considered concurrently with investigations into a new signalised crossing of Avoca Street which will generally facilitate improved traffic circulation within the RJTC.



Figure 89: Shared Street example (Greville Street, Prahran VIC)

### 6.5.4. Alison Road dining area (D)

The removal of the existing detracting commercial frontages along Alison Road (and the restoration of the two storey Victorian houses) would create several new ground level setback areas along the Alison Road frontage.

Given the northerly aspect and views to Alison Park, these new setback areas will be afforded increased amenity and will be well suited to alfresco dining opportunities. In addition, the increased building setbacks will enable a potential increase in street tree canopy planting.



Figure 90: Example pedestrian link space



### 6.5.5. Plaza Shopping Centre laneway and oval plaza (E)

The current vehicular and service access to properties with frontages to High Street (62-66) and Belmore Road (150-168) is directly to those streets, crossing busy existing footpaths. It is proposed to establish a new mid-block service vehicle laneway to avoid this unsafe conflict with pedestrian movement.

The new laneway would extent east from Clara Street, immediately north of the two heritage semi-detached houses at 17-19 Clara Street, ramping down to basement level to the rear of the High Street and Belmore Road properties. This new accessway would be implemented as a shared right-of-way along the south edge of the Randwick Plaza Shopping Centre property. At ground level (above the new service vehicle laneway) several pedestrian links are proposed, (refer Figure 93) leading to a central oval shaped mid-block plaza.



Figure 91: Plaza precedent

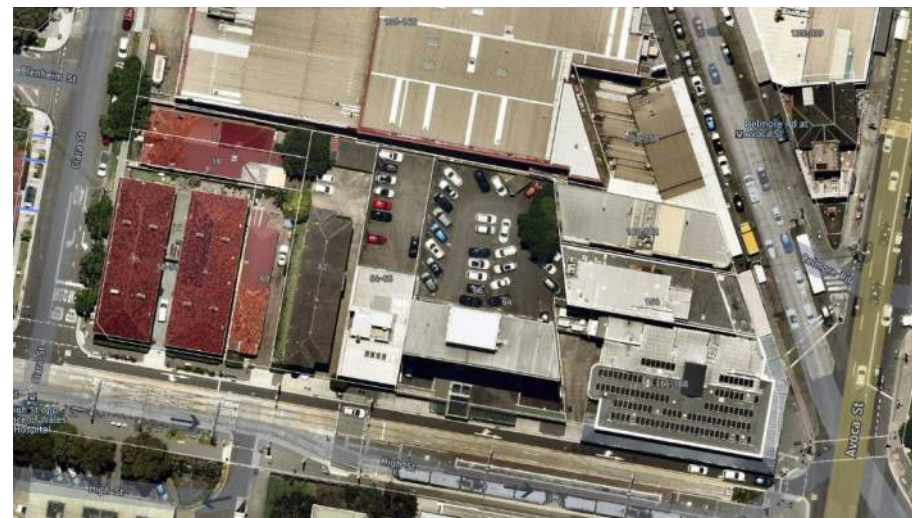


Figure 92: Existing vehicular access

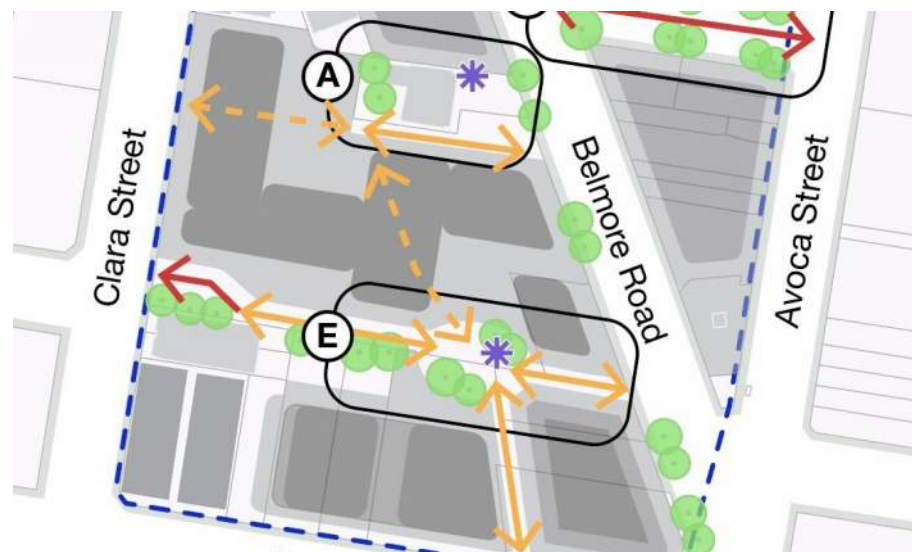


Figure 93: Plaza through site links



**Figure 94:** 3D model of potential plaza and through site links

### 6.5.6. Night-time activities (F)

The night-time activities within RJTC are currently focussed around the Coach and Horses Hotel and the adjoining restaurant strip. There is the opportunity to bolster the current night-time offering through the creative future adaptation of existing buildings.

An example of a converted heritage building is illustrated below, which utilises building setback areas for outdoor seating. In addition, the example utilises an adjoining right-of-way through temporary street closures to host special events. There is the potential for the former post office to contribute to events and night-time activities, utilising the setback areas and adjoining at-grade carparking area.

Additionally, Belmore Road serves as the primary spine of activity within the centre and presents a significant opportunity to strengthen its role as a vibrant night-time destination. Widened footpaths and existing mix of retail and hospitality venues provide a strong foundation for outdoor dining and activation. Enhancing the public realm through improved lighting, landscaping, and flexible seating arrangements could encourage extended trading hours and create a safer, more inviting environment after dark.



**Figure 95:** Converted heritage building example (The Posty, Richmond VIC)



## 6.6. Proposed planning controls

### 6.6.1. Proposed zoning

The E2 Commercial Centre zone is proposed to be extended to include four groups of existing residentially zoned sites (highlighted in red in the Proposed Zone Map) within the RJTC. The four groups of properties are listed below.

1. Numbers 119, 121, 123, 125 and 127-129 Alison Road (5 properties currently zoned R3 Medium Density Residential).
2. Numbers 7 and 9 Silver Street (currently zoned R3 Medium Density Residential).
3. 60 and 62 High Street (currently zoned R3 Medium Density Residential)
4. 144 Avoca Street (currently zoned R3 Medium Density Residential).

These properties are located on the edge of the existing town centre and are warranted for inclusion as they would better reflect the existing pattern of retail/commercial uses within RJTC. The zone extension will strengthen the centre by increasing opportunities for additional business uses/mixed uses and furthermore, contribute to and support the town centres future growth.

In addition to the E2 zoning, it is proposed to rezone Waratah Plaza to RE1 Public Recreation to formalise this civic space as public open space.

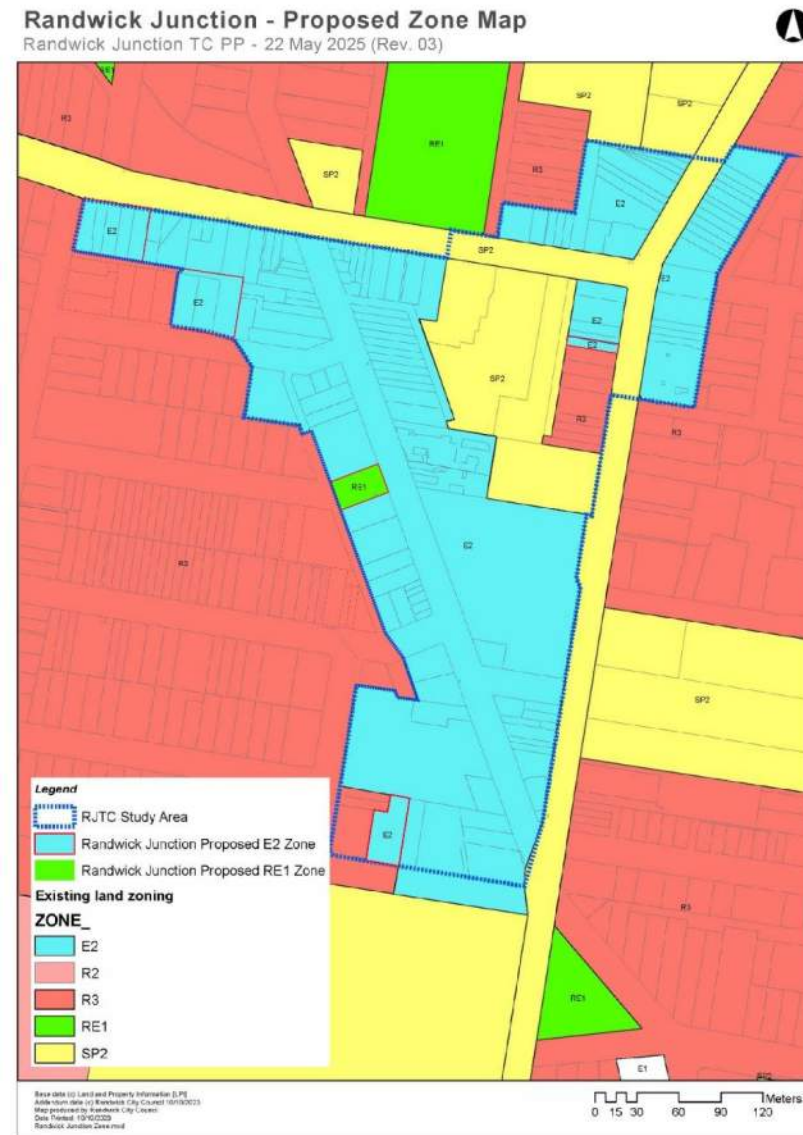


Figure 96: Proposed Land Zoning map

### 6.6.2. Proposed density

The proposed Floor Space Ratio (FSR) is derived from the total floorplate area that resulted from the extensive 3D modelling process and the urban design analysis.

The Gross Floor Area (GFA) component of the FSR was calculated using a 70% floorplate efficiency for all levels and uses. While higher efficiencies might be expected for commercial uses and in some residential uses, the more conservative 70% figure was utilised in the calculations for the following reasons:

- **Heritage** – As the majority of the RJTC is within a Heritage Conservation Area and there are numerous heritage items and contributory items, the ability to achieve high floorplate efficiencies could potentially be limited by the need to retain and protect heritage built fabric
- **Site proportions** – Many properties within RJTC are of narrow or obtuse dimension, limiting floorplate area and potential design efficiencies
- **Further design controls** – While the 3D model does take into consideration Apartment Design Guide (ADG) setbacks and heritage input from City Plan Heritage, the modelling only represents Council's desired maximum built form massing, it does not take into consideration any Development Plan Control (DCP) articulation and design requirements, that may reduce floorspace area.

Justification for the new maximum FSR is based on the Built Form Framework as follows:

#### Strategic Sites

These sites have larger floor plates, and relatively limited constraints to redevelopment. The proposed density for the Strategic Sites are the highest proposed within RJTC, owing to orientation and location that enables taller built form to be achieved without substantial offsite impacts.

#### Heritage and contributory buildings

Heritage items that are on the State Register or items listed under the Randwick LEP and that are seen as inappropriate to allow any uplift above the current height and density controls – items of high sensitivity or that are experienced 'in the round' will have no change to their development controls.

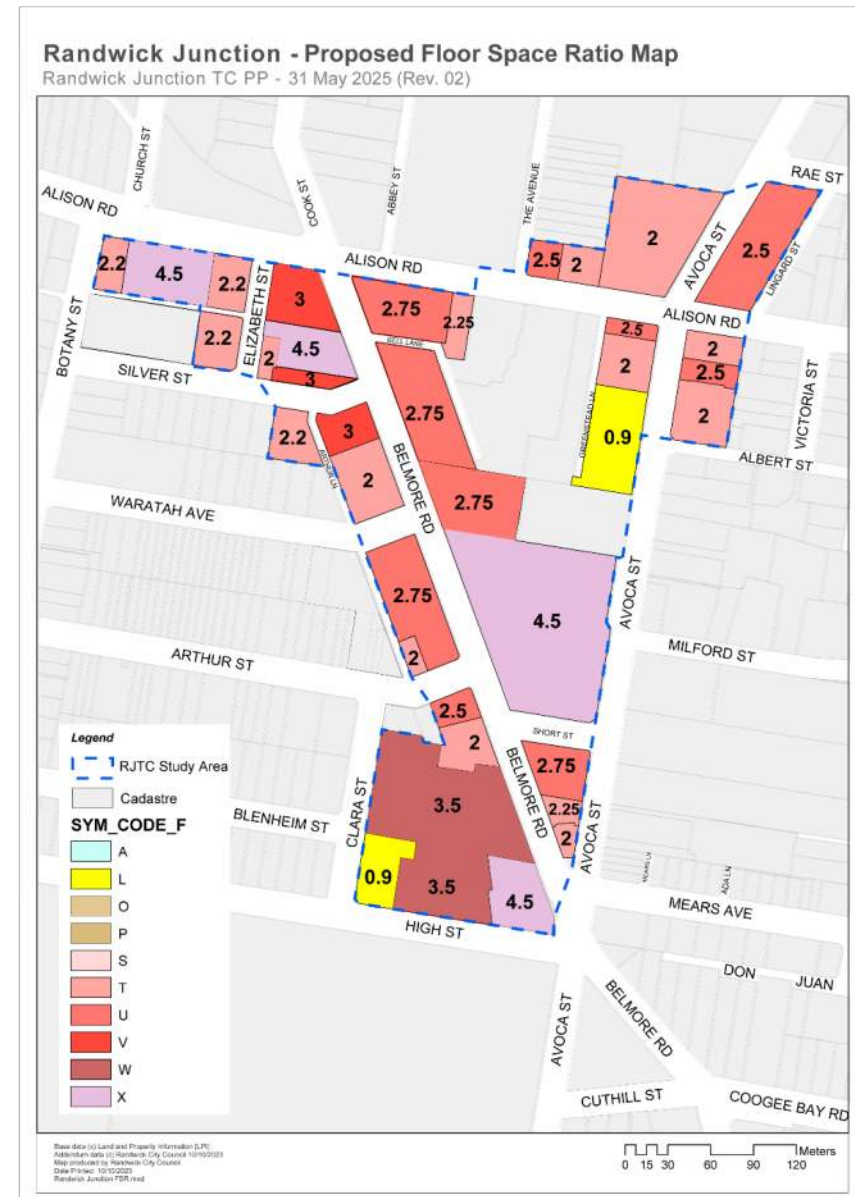


Figure 97: Proposed Floor Space Ratio map

It is proposed to make marginal changes to the FSR of heritage and contribution items where it is considered appropriate to accommodate small scale additions/extensions. Owing to the need to sensitively address heritage criteria, some FSR for heritage items and some contributory buildings are proposed to be retained at their current levels.

#### Infill sites

These sites generally are characterised by diverse periods of historical development, including Victorian terraces through to 1970s residential flat buildings. Most of these sites are capable of moderate increases in density to accommodate substantial alterations/additions. It is proposed that infill sites generally adopt an increased FSR between 2:1 and 2.75:1.

### 6.6.3. Proposed non-residential FSR

It is proposed to introduce a non-residential FSR, which is the ratio of non-residential floor area (GFA) to the site area, for all Strategic Sites.

Requiring increased commercial floor area in these locations, will foster vibrancy and support the needs of the growing health and education precinct. The draft RDCP will reinforce these requirements by specifying minimum floor-to-floor heights.

This non-residential FSR control has been informed by the Randwick Junction Economic and Feasibility Analysis (SGS, April 2023 and 2025) and is guided by the advice provided therein. The non-residential FSR has generally been established based on a 70% floorplate efficiency, such that the non-residential floor space will be delivered over one, two or three floors of new buildings depending on the location. In addition, this will ensure that commercial floorspace is delivered at key locations within the town centre.



Figure 98: Proposed Minimum Non Residential FSR map



It is proposed to amend the maximum permitted RLEP HOB Map for properties in the RJTC. The proposed changes to maximum building height have been based on results from extensive 3D modelling, key view analysis and architectural heritage reviews to ensure that the proposed increased building heights and built form is appropriate for the heritage context of the town centre.

- Ground - 4.4m
- First - 3.7m
- Second storey and above - 3.1m

– HOB of 12m	3 storeys
– HOB of 15m	4 storeys
– HOB of 18m	5 storeys
– HOB of 21m	6 storeys
– HOB of 36m	11 storeys
– HOB of 39m	12 storeys
– HOB of 40.5m	12 storeys (with commercial podium)
– HOB of 42m podium)	12 storeys (allowing for a 2 storey, 5.5m fl-to-fl retail podium)
– HOB of 45m podium)	13 storeys (allowing for a 2 storey, 5.5m fl to fl retail podium)

The Royal Randwick Shopping Centre and Randwick Plaza Shopping Centre sites have a 5.5m floor-to-floor height for their ground and first floors, and 3.1m floor-to-floor height for the second floor and above resulting in a 45m HOB. The allowance for increased height for the shopping centre sites enables two taller retail 'shopping centre' floors.

**Legend**

- RUTC Study Area
- Proposed change to HoB
- Cadastral

**SYM\_CODE\_H**

M	W
O	X
P	
R	
S	
T	
V	

Map showing the Rujic Transport Corridor (RUTC) Study Area in Randwick, NSW, with proposed changes to the Heritage Overlay (HoB). The map displays various land parcels with their SYM\_CODE\_H values and colors. The legend indicates the RUTC Study Area (dashed blue line) and the Proposed change to HoB (red outline). The SYM\_CODE\_H values are color-coded: M (yellow), O (orange), P (brown), R (dark brown), S (light pink), T (red), and V (dark red). The map also shows surrounding streets and a scale bar in meters.

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### 6.6.5. Proposed active frontages

The requirement for active frontages to the various streets within the town centre will ensure street vibrancy, liveliness, and safety. The Randwick LEP active frontage provision will require that sites provide ground floor commercial or retail floor space.

A Randwick DCP control is also recommended to encourage developments to provide active frontages to mid-block links, secondary streets and laneways where active frontages are preferred (rather than mandatory). The DCP will be reviewed and drafted for Council's consideration based on the recommendations of this Urban Design Report.

### Randwick Junction - Proposed Active Frontages Map

Randwick Junction DCP - 31 May 2025 (Rev.03)

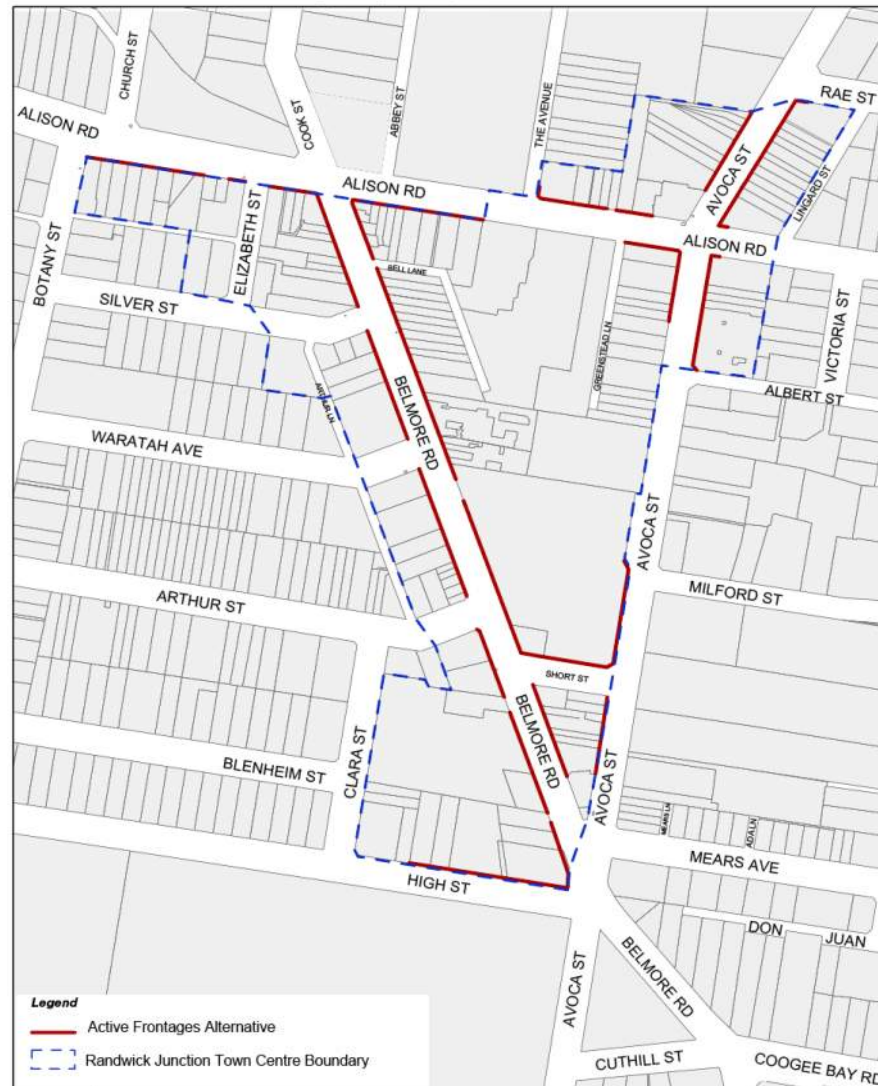


Figure 100: Proposed active frontages map

### 6.6.6. Affordable housing

It is important for sites in the town centre that are receiving an uplift in density (FSR) to assist in the delivery of affordable housing for the community. The Randwick LEP Special Provision Map will require that these sites deliver a component of affordable housing that is consistent with the Randwick Junction Economic and Feasibility Analysis (SGS, April 2023 and 2025).

Randwick Junction TC PP - 6 June 2025 (Rev. 03)

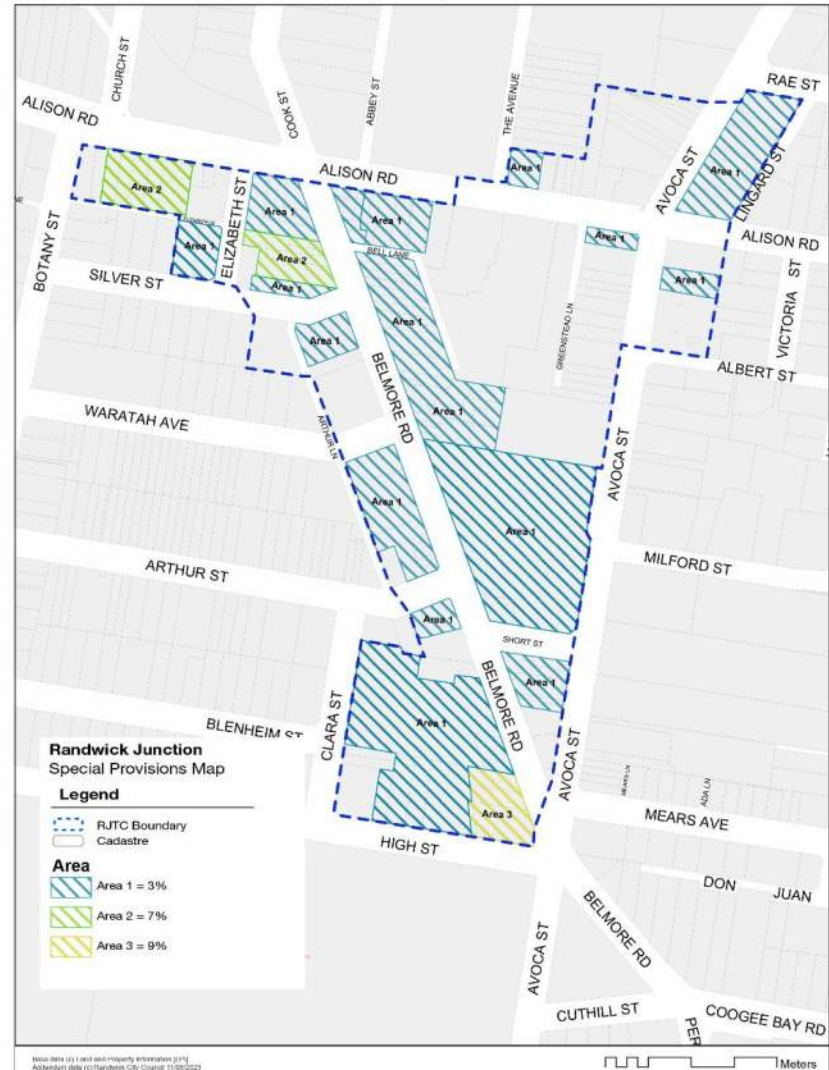


Figure 101: Proposed Special Provisions



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## 7. Conclusion

### Introduction

Randwick Junction Town Centre (RJTC) is unique among the commercial centres in the Randwick LGA, as the Randwick Junction Heritage Conservation Area extends across most of the town centre and its character is defined by over 30 heritage buildings and over 40 contributory buildings.

There is a well-established 'fine grain' urban structure to the town centre, and it is this quality that gives the centre's streets their small scale, variety and interest, and the 'village' like character that the community values.

RJTC is well served by public transport, a vibrant mix of retail shopping (including two major shopping centres) and the town centre adjoins two major employment hubs – the UNSW and the Randwick Hospital campus. It is therefore reasonable in the planning of the town centre to seek opportunities for moderate growth in the centre's commercial, retail and residential capacity over the next 15-20 years.

A strategic approach has been taken to the planning of the town centre, including looking for opportunities to incentivise good urban planning outcomes, such as the redevelopment of detracting buildings and the provision of new public places, pedestrianised local streets and laneways and the creation of small urban plazas in public street closures (Waratah Avenue) and in areas of deep soil provision within private development sites.

The planning strategy protects existing State and local heritage listed properties and encourages their restoration. The urban planning challenge has been to plan for a moderate uplift on suitable heritage properties, and appropriate uplift on the less constrained Strategic Sites, whilst also ensuring that any redevelopment is feasible. In this regard the study was informed by two important specialist consultant studies – a Heritage Assessment (City Plan Heritage, Feb 2023) and an Economic and Feasibility Analysis (SGS, April 2023 and 2025).

The study identified a series of potential future public domain upgrade projects that would improve the amenity and attractiveness of the town centre to visitors, workers and residents. There are opportunities for health and medical and

innovative start-up businesses to locate in the town centre, leveraging and supporting the growth of Randwick Hospital and the UNSW.

This RJTC Urban Design Report recommends a considered and balanced urban planning approach to guide the growth of the town centre over the next 15-20 years.

### Recommendations

The specific recommendations of the study are:

#### Strategic planning

- Encourage renewal of the north of the town centre and bring more activity by permitting additional height and density for the two Strategic Sites – The Randwick Club and the Former CBA Site, encouraging the business or club activities to be incorporated in any proposed redevelopment, and encourage the renewal and renovation of the 187-181 Alison Road properties to create a new alfresco dining destination overlooking Alison Park
- Encourage health, medical and innovative start-up businesses to locate in the town centre, including along High Street, leveraging and supporting the growth of Randwick Hospital and the UNSW
- Develop an Affordable Housing Plan for RJTC

#### Randwick LEP

Some rezoning of sites at the periphery of the town centre is proposed to accommodate growth and to rationalise the existing town centre business zone.

Further, it is recommended that there should be no increase in height or FSR above the existing 12m HOB and FSR 2:1 on the highly valued State and local listed heritage buildings in the town centre. Moderate increases in height and FSR are proposed for properties with heritage or contributory buildings (based on specialist heritage consultant advice) where sites are considered appropriate to accommodate small scale increases of up to three storeys. On the less constrained and larger Strategic Sites moderate uplift in height and density is proposed.

- Zoning – extend the E2 Commercial Centre zone to include four groups of existing R3 zoned sites as per the Proposed Zone Map
- Floor Space Ratio (FSR) – increase the FSR on those sites indicated on the Proposed Floor Space Ratio Map

- Non-residential FSR – require a minimum non-residential FSR on those sites indicated on the Proposed Minimum Non-Residential FSR Map
- Height of Building (HOB) – increase the HOB for those sites indicated on the Proposed Height of Building Map
- Active Frontages – require active frontages to various streets and laneways in the town centre to ensure street vibrancy, liveliness and pedestrian safety on those sites indicated on the Proposed Active Frontages Map
- Affordable Housing – require new developments to contribute to the delivery of affordable housing according to the percentage levy indicated on the Proposed Special Provision Area Map

#### Design excellence

- All strategic sites are required to present to the design excellence advisory panel and the findings of the Panel are to be considered at Development Application (DA) stage by Council development assessment officers
- 4 Star Green Star certification is recommended as a requirement for all Strategic Sites

#### Pedestrian access

- Improve the cohesiveness of the overall town centre by improving the east-west pedestrian connections and through site pedestrian links between Belmore Road and Avoca Street, investigating a new signalised intersection at the Avoca Street/Milford Street intersection and facilitating ‘stepping-stones’ of activity at Alison Road and Short Street
- Wherever possible consider the widening of footpaths, reducing carriageways to the minimum standard, to enhance the safety and amenity of pedestrians moving through the town centre

#### Public domain upgrades

- June Moore Place – upgrade and renew the landscaping of this important pocket plaza off the Belmore Road Main Street
- East House (former Post Office) – upgrade the footpaths and associated setback areas with new paving, lighting, landscaping, street furniture and heritage interpretive signage providing new green places for people to enjoy in the town centre

- Short Street – investigate a one-way shared zone, prioritising pedestrian access and provide alfresco dining areas and new street landscaping, providing new green places for people to enjoy in the town centre
- Elizabeth Street – investigate a one-way shared zone, prioritising pedestrian access and provide outdoor dining areas and new street landscaping, providing new green places for people to enjoy in the town centre
- Alison Road dining area – as part of redevelopment of these sites, remove the existing detracting commercial frontages and restore the two storey Victorian houses behind, creating a new north oriented alfresco dining area destination overlooking Alison Park
- Laneway activation – in Arthur Lane and Bell Lane, establish a pedestrian friendly shared zone with new landscaping, lighting, street furniture and public artwork, and encourage businesses to provide active retail/commercial frontages to the laneways to support pedestrian activity and events
- Night-time hub – consider ways to encourage the night time hub around the Coach and Horses Hotel and restaurant strip, including establishing a unique identity and branding and reinforcing the new identity through the coordinated upgrade to the paving, lighting and street furniture of this northeast area of the town centre
- Lighting strategy – Prepare a lighting strategy for the overall centre, with particular focus on heritage items and contributory buildings along with night-time venues including laneways

#### Heritage

- As required under the LEP, if heritage listed buildings or contributory buildings form part of a redevelopment proposal a Heritage Assessment or Heritage Impact Statement is to be prepared for the property, considering the proposed redevelopment proposal prior to any major works being undertaken
- Further, at the discretion of Council’s Heritage Planner, a Conservation Management Plan or Strategy (CMP or CMS) may be required prior to the undertaking of any physical works to ensure the future management and conservation of the heritage item or contributory building is in accordance with Heritage NSW requirements.
- Incentivise the removal of detracting commercial buildings along Alison Road and the restoration of two original houses and Montrose House for

restaurant/café uses with widened north oriented alfresco dining areas overlooking Alison Park

- Require the restoration, including the removal of disused or poorly designed/located signage, and the replacement of inappropriate window frames and other details, and repainting of heritage buildings to their original colour scheme based on expert heritage advice
- Preserve heritage or contributory building roofscapes/chimneys
- Strengthen the Randwick DCP development controls, requiring developments to address the maintenance and restoration of heritage and contributory buildings.

#### Environmental and visual amenity

- Consider a public artwork strategy and plan with focus on aboriginal history and perspectives
- Contribute to the Randwick LGA 22% tree canopy target by exploring opportunities for planting appropriate species of street trees where awnings do not preclude plantings
- Consider introducing for redevelopment in the town centre a minimum percentage Landscape Area requirement, similar to that applying in the Kensington and Kingsford Town Centres DCP
- Plan for the progressive removal of overhead power lines and timber poles, by undergrounding the power supply – consider a new Smart Pole standard for lighting the town centre

#### Night time economy

- Encourage the night-time economy, through the renewal of sites and existing venues clustered around the Coach and Horses Hotel including the Avoca Street restaurant strip and former Post Office building and the branding and promotion of the night time destination.

#### Transport, traffic and parking

- Continue to liaise with Transport for NSW (TfNSW) regarding timing for business case studies into the provision of a Metro line and Randwick Station, and a Rapid Bus routes along High Street and Avoca Street, including potential bus-light rail transport interchange
- Investigate implementation of a 30km/h High Pedestrian Activity Area (HPAA) for the Belmore Road main retail street from Avoca Street intersection (south extent) to the Alison Road intersection (north extent), in partnership with TfNSW, including appropriate signage and visual cues at the thresholds and traffic calming street design measures
- Investigate the potential to relocate bus stops from Belmore Road to Avoca Street to improve the pedestrian amenity of the town centre Main Street (Belmore Road)
- Explore opportunities for an additional signalised pedestrian crossing of Belmore Road between Waratah Avenue and Silver Street, and footpath widening and new street trees along the west side of Belmore Road (the east side being impacted by afternoon peak hour parking restrictions) to improve pedestrian safety, amenity, and streetscape visual appeal
- Liaise with TfNSW and the owners of Royal Randwick Shopping Centre regarding the installation of a new signalised pedestrian crossing of Avoca Street at or near to the Milford Street intersection, incorporating a southbound right hand turn into the shopping centre carpark, to avoid unnecessary circulation of vehicles via the Belmore Road 'Main Street'
- Develop a public parking management plan for RJTC
- Amend the Randwick DCP to reduce car dependency and ownership rates, by applying to redevelopment in RJTC, new car parking rates at one third reduced rates on the TfNSW standard rates as recommended in the Stantec Transport Report.



